

An Industrial Heritage Survey of Railways in Counties Monaghan and Louth Part 2



SITE INVENTORY: VOLUME 2.1

Dundalk - Enniskillen Line (Sites 03101-03542)

for
Monaghan County Council and Louth County Council
December 2007



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INTRODUCTION

This is the second volume detailing all railway-related sites in counties Louth and Monaghan. This particular volume focuses on those sites on the Dundalk-Enniskillen line which lie within counties Louth and Monaghan. Sites 03101 - 03224 inclusive are in Co Louth and the rest in Co Monaghan.

The inventory is divided into three parts. The first notes the line's salient historical details and general route.

The second part details notable stretches of track bed as follows:

Stretch	Number (according to line, section, and sequence within section).
Type	Each stretch comprises one or more of a Cutting, Embankment or Level (i.e. neither a cutting nor embankment).
Length (m)	Length of stretch in metres.
Description	Outline of stretch characteristics.
[Photographs]	These are contemporary photographs of the site and are listed by Image Number. They begin with general site views, followed by those for each of its components in sequence.
	Photographer: FWH = Fred Hamond.
	Date of photograph.
	Caption.

The third part describes each site:

Site	Number (according to line, section, and sequence within section).
Name	Site name (where known). In the case of stations, their abbreviated names as given in the Working Timetables are also noted in brackets.
History	Overview of site's development.
[Historical photographs]	Image number (by site number and image sequence within site).
	Photographer.
	Date of photograph (where known).
	Source of photograph.
	Caption
	Only photographs for which permission has been granted are depicted.
[Map]	In the case of stations, a 25-inch (1:2500) map showing the location of all site components is included.

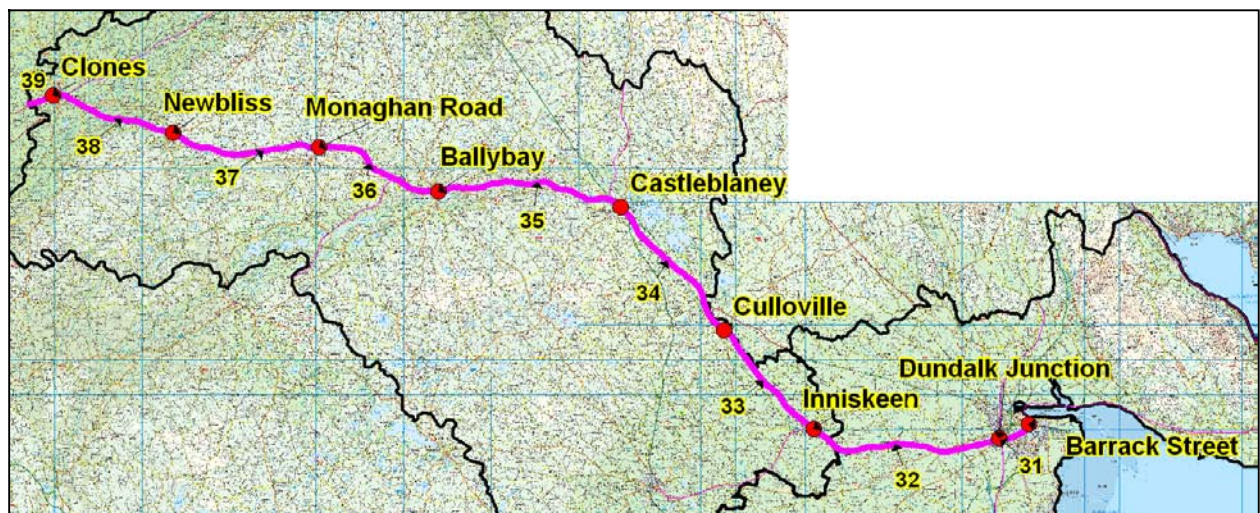
Site component – all components within the site are itemised in numerical sequence	
Component	Numerical suffix to site number.
Type	Type of component.
County	Louth or Monaghan
OS 6-inch map	County abbreviation + map sheet number.
Grid	Easting and northing to nearest 1m.

Surveyed?	Y = Yes; N = No.
Survey date	Day/ Month/ Year.
Surveyor	Fred Hamond. AR&R = Architectural Recording & Research.
Completeness	No visible remain[s]. Traces. Some remains. Substantial remain[ns]. Complete.
Condition	Good = Maintained. Fair = Sound but not maintained. Poor = Unsound. N/A = Not applicable where no remains survive.
Use	Present use (not applicable in case of sites with no remains).
Description	Details of form, materials, embellishment etc.
Interest categories	The following National Inventory of Architectural Heritage categories are used: Architectural, Historical, Social, Technical, Group, Setting and Uniqueness/Rarity.
Evaluation	Assessment of industrial heritage significance based on component attributes which match the above categories of interest.
Significance	Record Only = No special significance. This applies to components which have no upstanding remains and to those where the remains are of no special interest. Otherwise, the site is of Local, Regional or National industrial heritage significance.
Protection	Level of statutory protection currently applied to site. Protection generally takes the form of inclusion in the Record of Protected Structures (RPS), the list number of which is also given.
Action	Recommendations for further action (such as including in RPS).
Other databases	Other databases in which this site component is included. This is usually the National Inventory of Architectural Heritage (NIAH), the inventory number of which is also given.

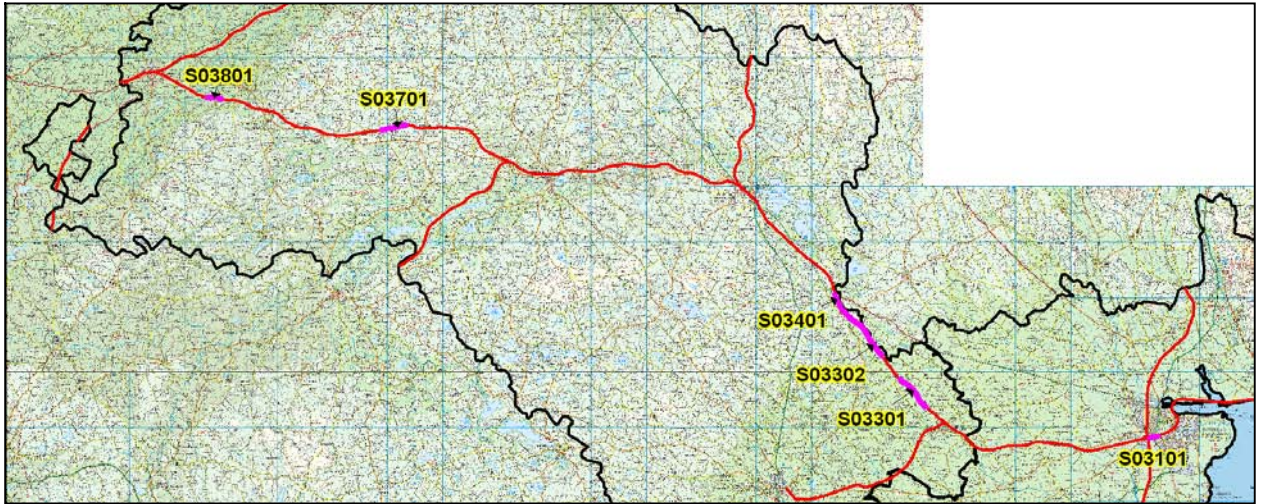
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	Date of photograph.
	Caption.

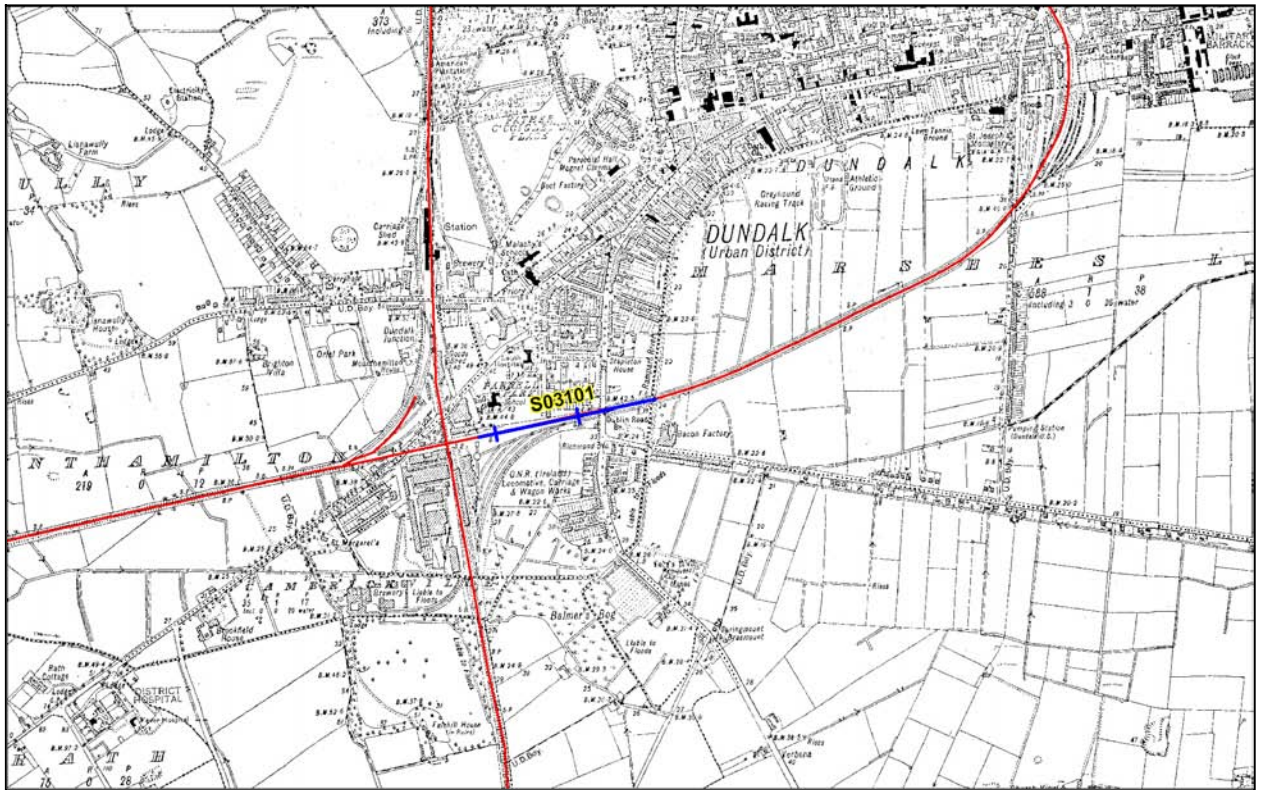
3 DUNDALK-ENNISKILLEN LINE

From/To	Dundalk/ Enniskillen (Co Fermanagh)
Opened	To Castleblaney 15 February 1849 To Ballybay 17 July 1854 To Newbliss 14 August 1855 To Clones and Lisnaskea 7 July 1858 To Enniskillen 15 February 1859
Opened by	Dundalk & Enniskillen Railway
Total length	101 km
Length in study area	67.1 km
Length in Co Louth/ Monaghan	12.7 km/ 54.4 km
Operated by	Dundalk & Enniskillen Railway; Irish North Western Great Northern Railway (Ireland)
Closed to passenger traffic	13 October 1957
Closed to goods traffic	31 December 1959

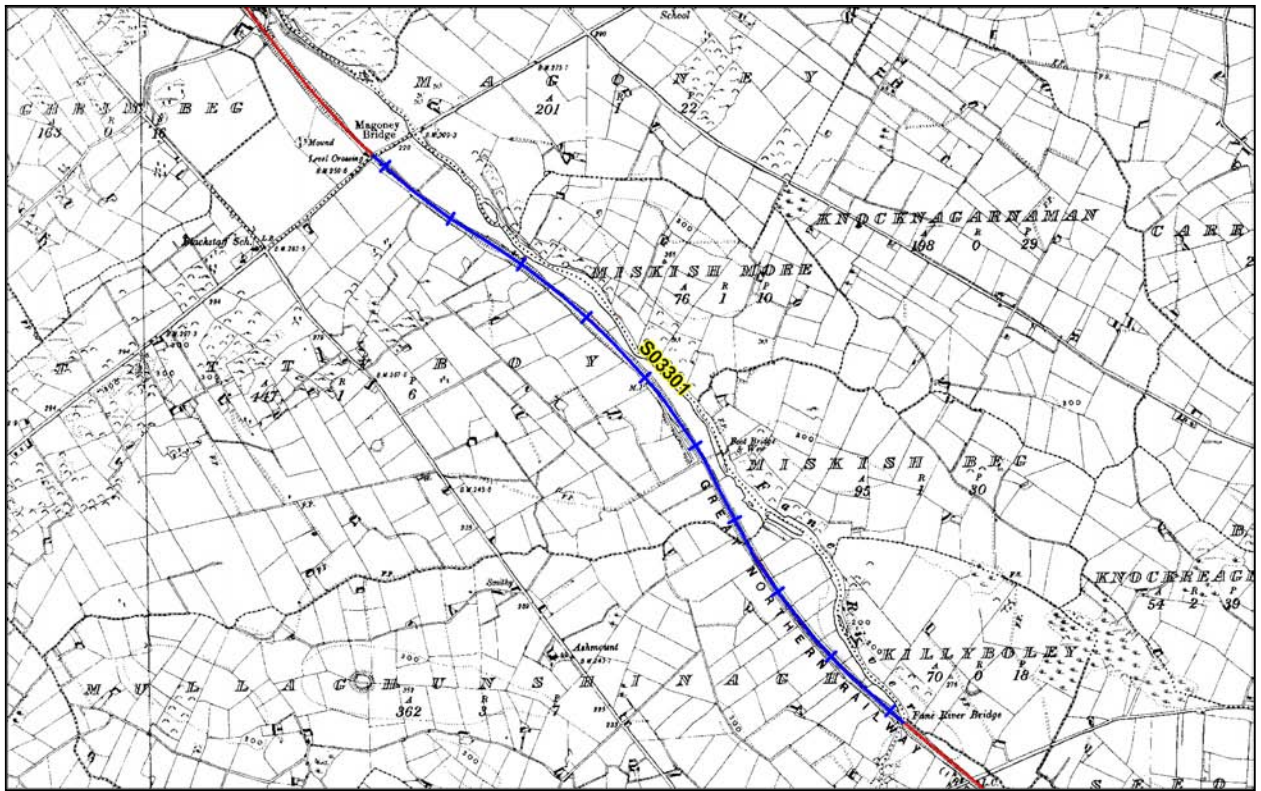


Line 3: Stretches of interest

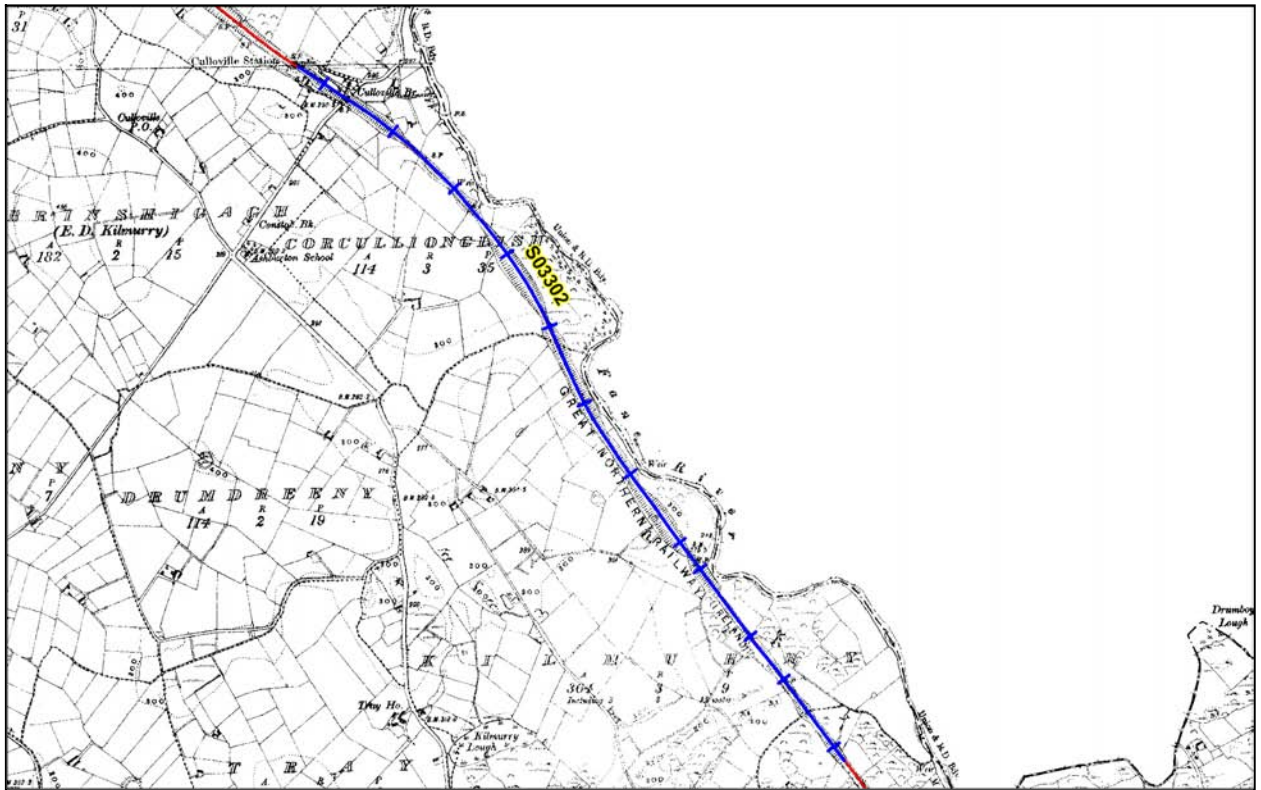




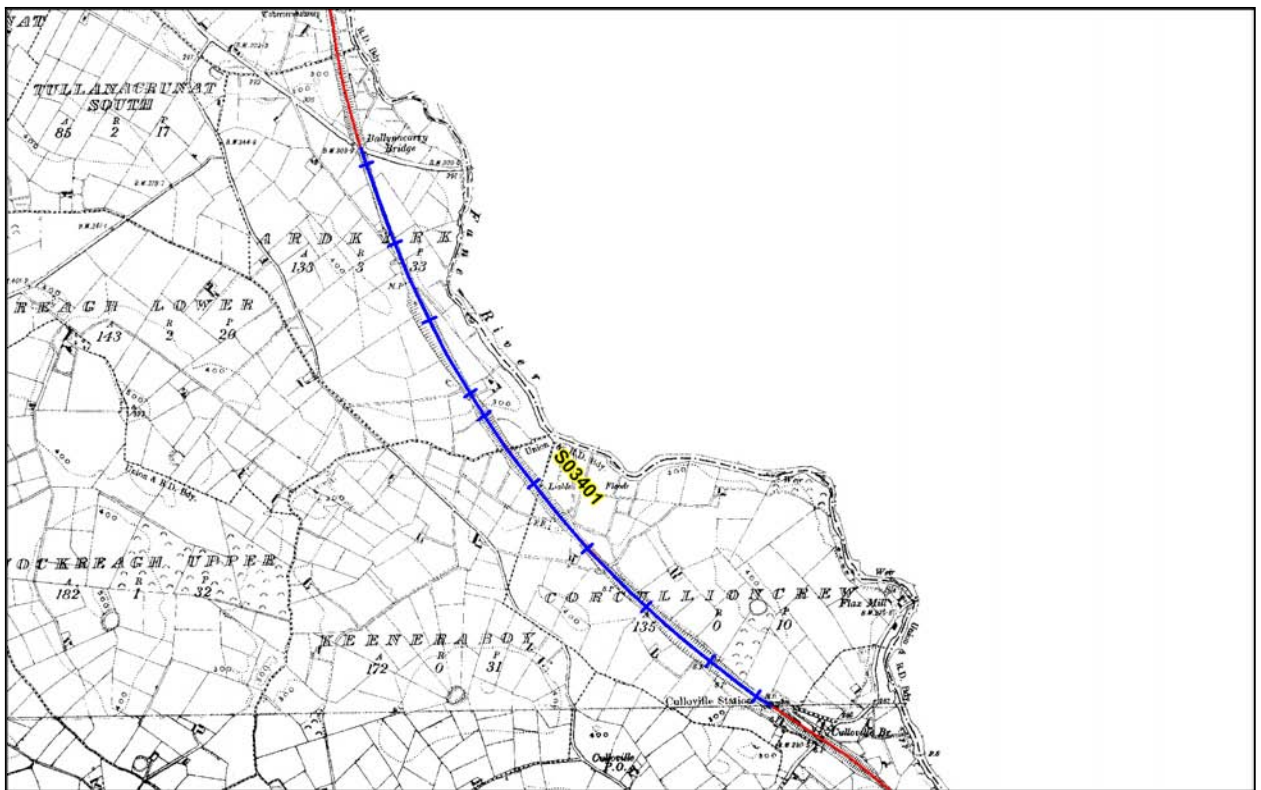
Section S03101



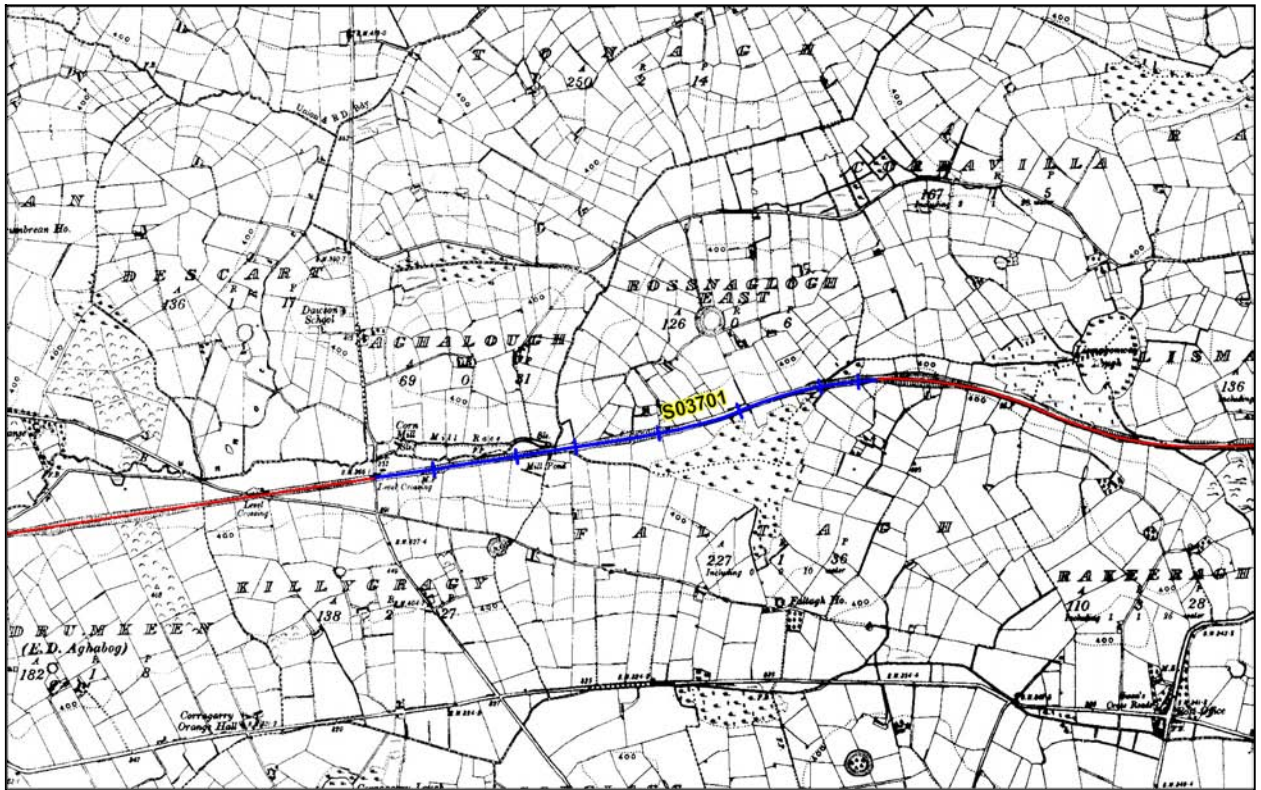
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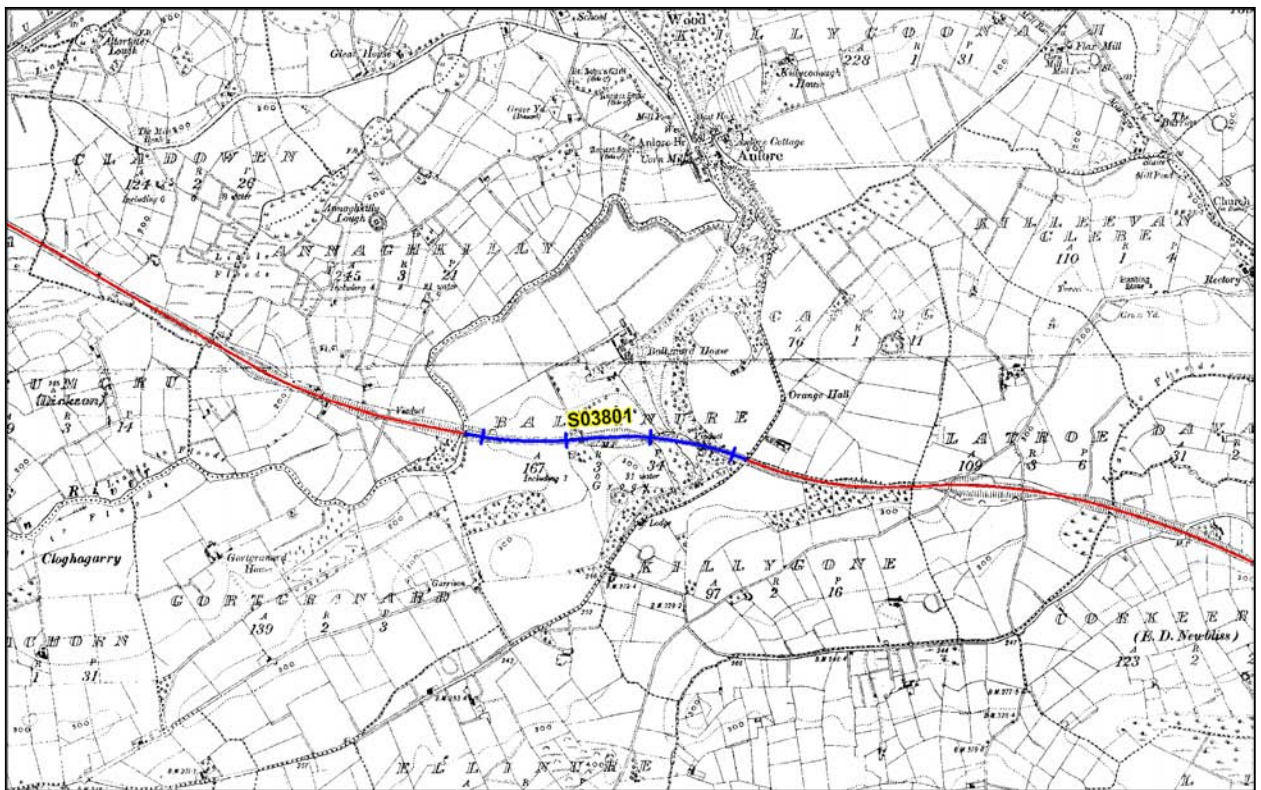
Section S03302



Section S03401



Section S03701



Section S03801

Stretch **S03101** **Type** Level **Length (m)** 479

Description A section of flat track survives between the former footbridge over the line (03105) to just before the Square Crossing (03109). Although the actual line has been lifted, this stretch remains undeveloped and clear of obstructions. It reflects the scale of the railway and its associated link with the main Dublin-Belfast line and siding into the engine sheds within the GNR Works (01402).

S03101_01 FWH 31/07/2007 Looking east from Dublin Road Bridge (3107).



S03101_02 FWH 31/07/2007 Looking west from Dublin Road Bridge (3107).



Stretch **S03301** **Type** Cutting; Embankment; Level **Length (m)** 2088

Description The Monaghan Way runs along this stretch between the Fane River Bridge (03305) and former Blackstaff Halt (03310). Although the line has been reclaimed in places and is no longer clearly evident on the ground, this is a scenic section along the right bank of the Fane.

Stretch **S03302** **Type** Cutting; Embankment; Level **Length (m)** 2377

Description The Monaghan Way runs from just after feature 03314 to Culloville Station (03401). The first section of the line has been reclaimed but an intact stretch runs from 03315 onwards. This section takes in the remains of a level crossing (03316) and rail/road bridge (03317).

Stretch **S03401** **Type** Cutting; Embankment; Level **Length (m)** 1878

Description The Monaghan Way continues from Culloville Station (03401) to the main road at the former Ballynacarry Bridge (03406), running on or beside the former track bed. The best stretch is the deep cutting running north-west from the station, along which a farmaced accommodation road now runs under a triple-span metal beam bridge (03402).

S03401_01 FWH 09/08/2007 Looking NW along cutting from station.

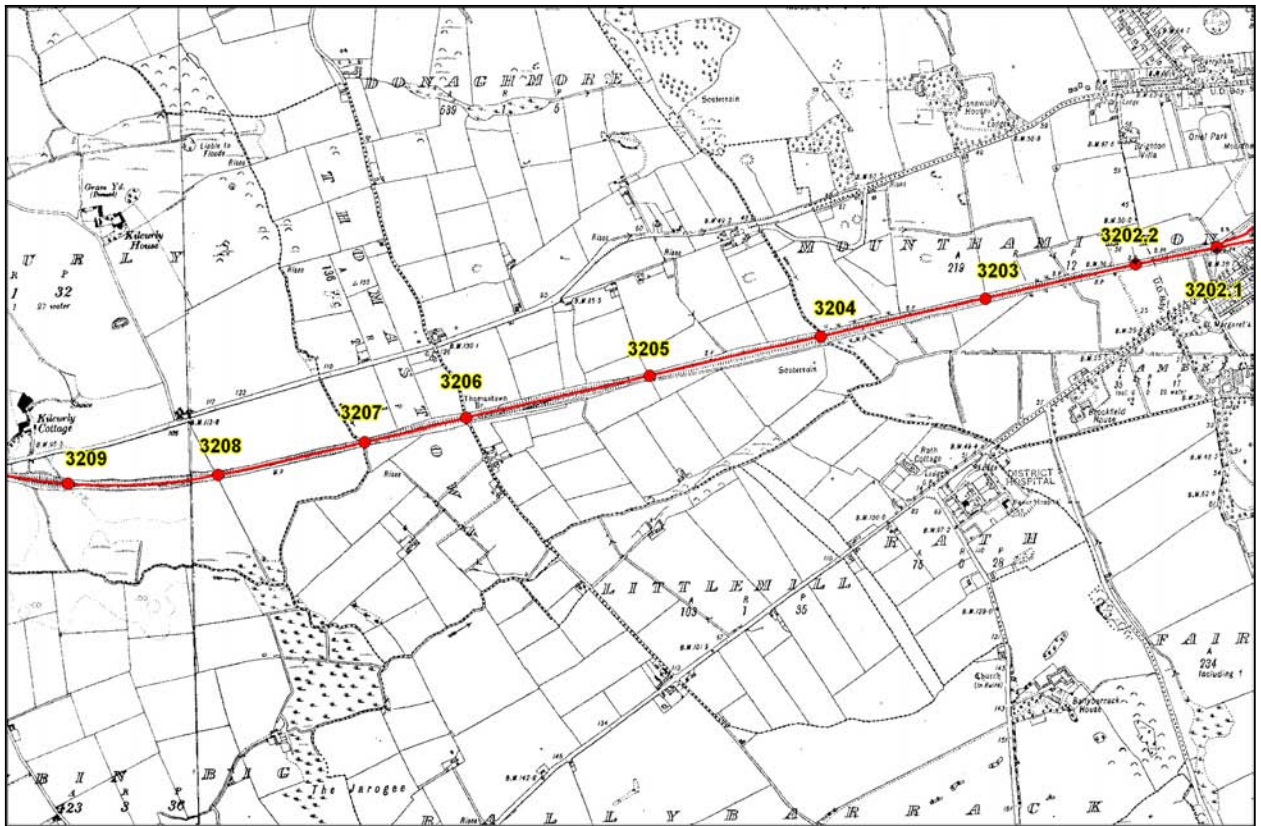
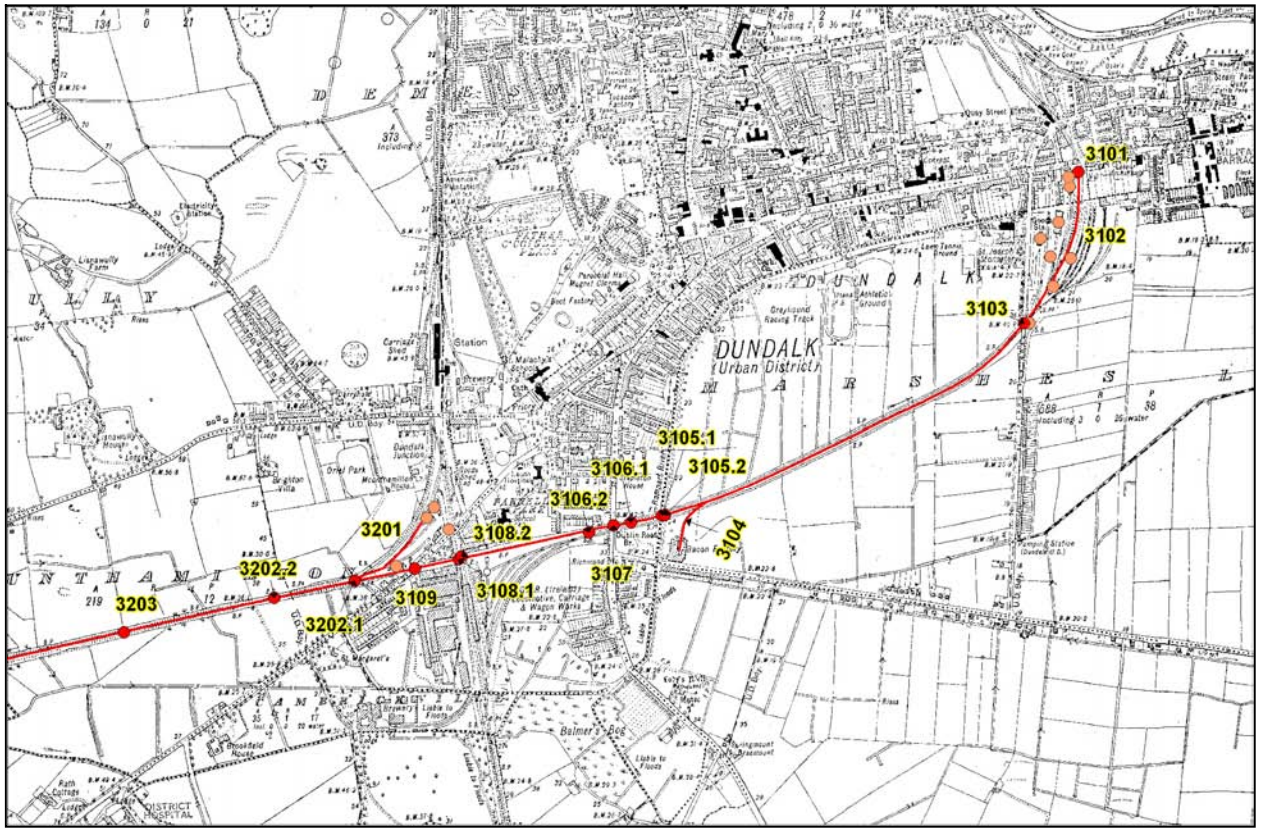


Stretch **S03701** **Type** Level **Length (m)** 1355

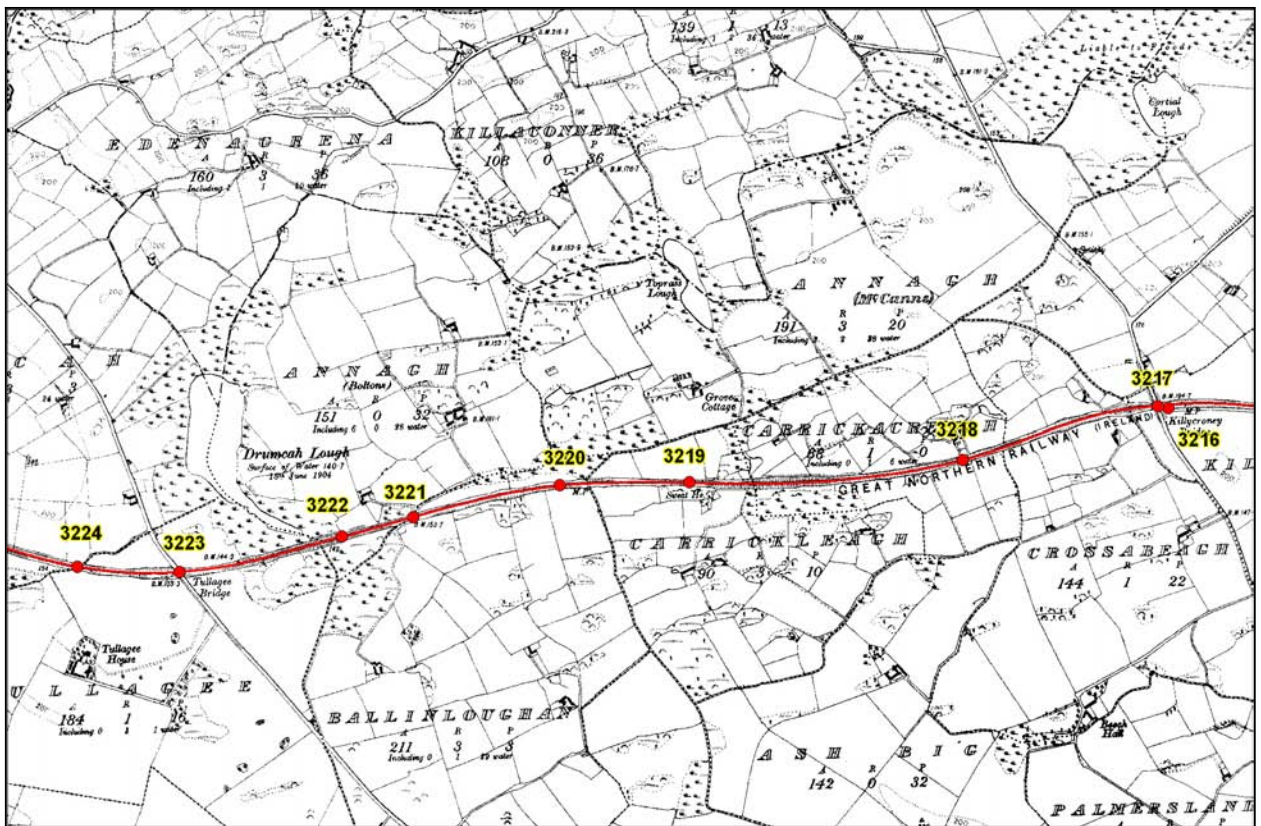
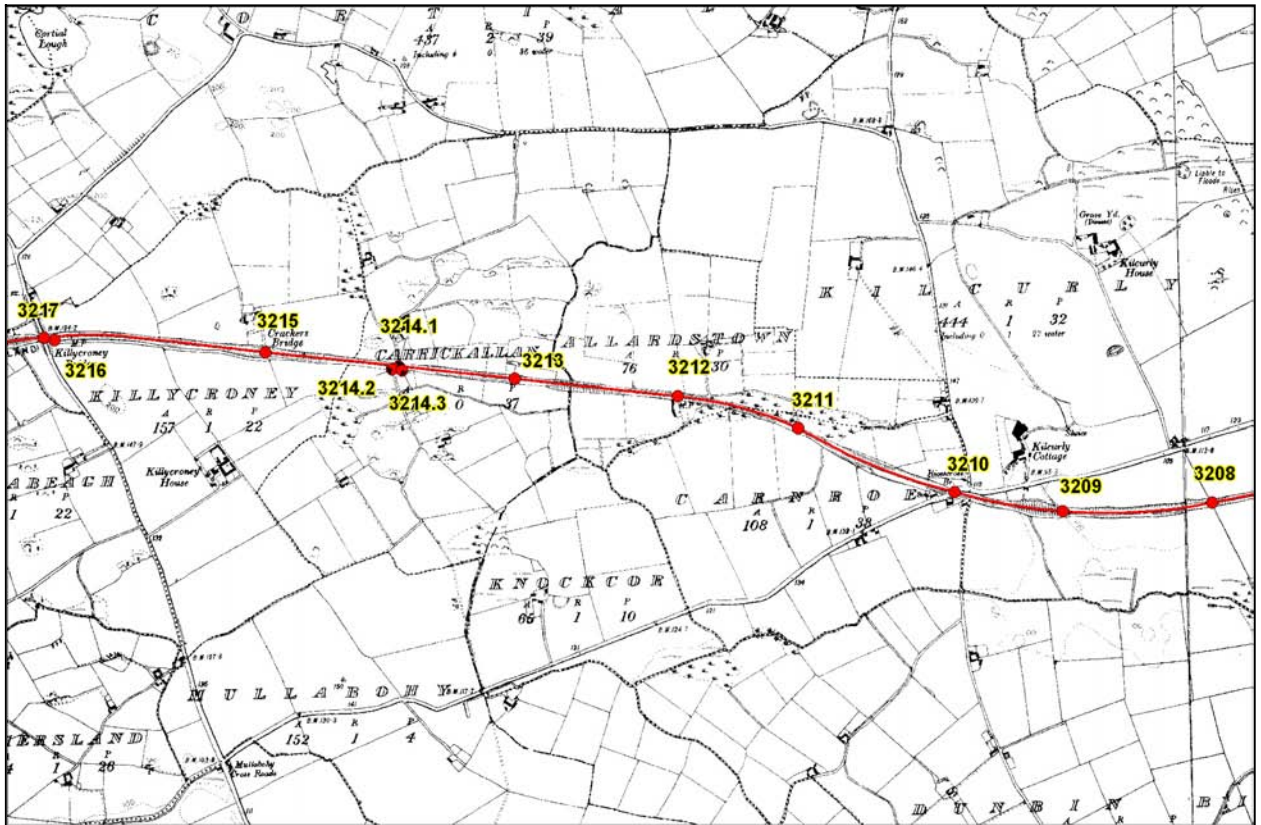
Description This section of line, some distance west of Monaghan Road Station to Killygragy Crossing (03705), has an accommodation track running along it. It is mostly flat, without embankation and with only minor cuttings.

S03701_01 FWH 11/10/2007 Looking west along track bed from feature 3704.

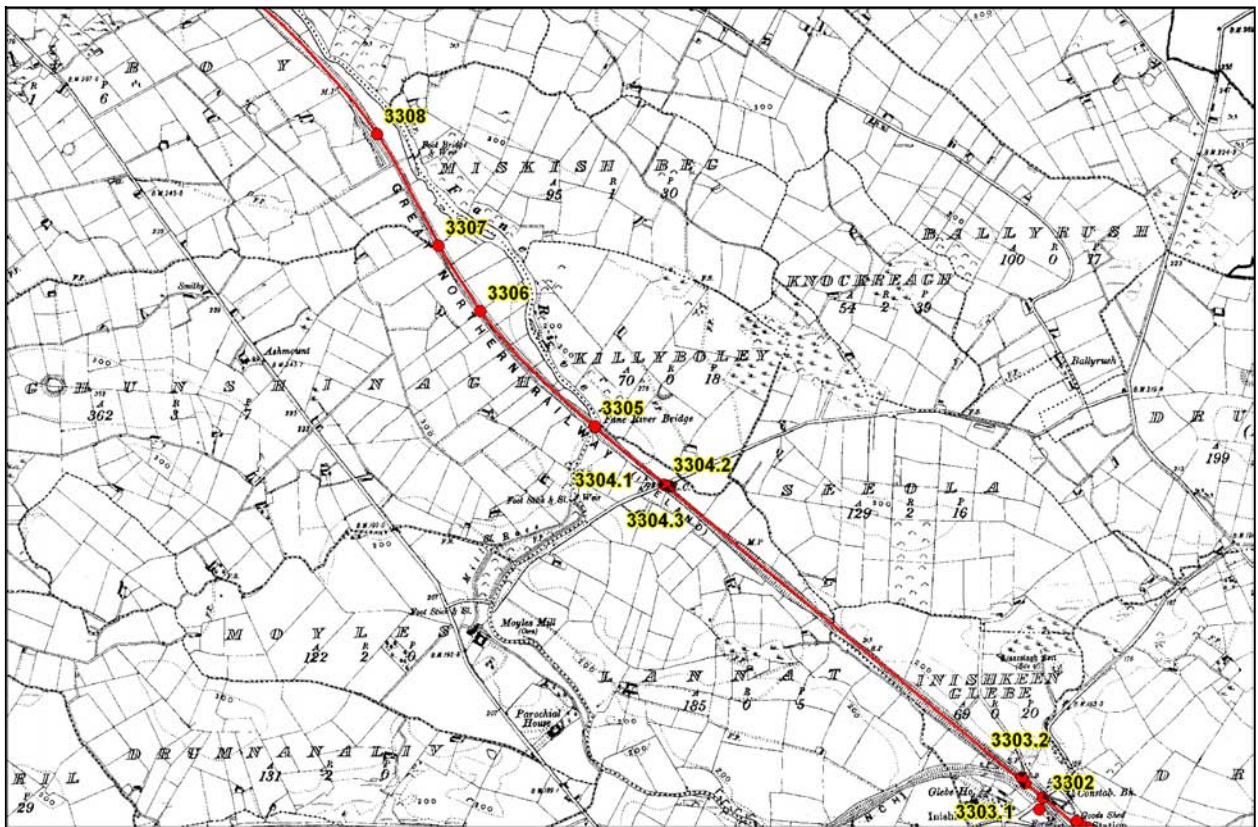
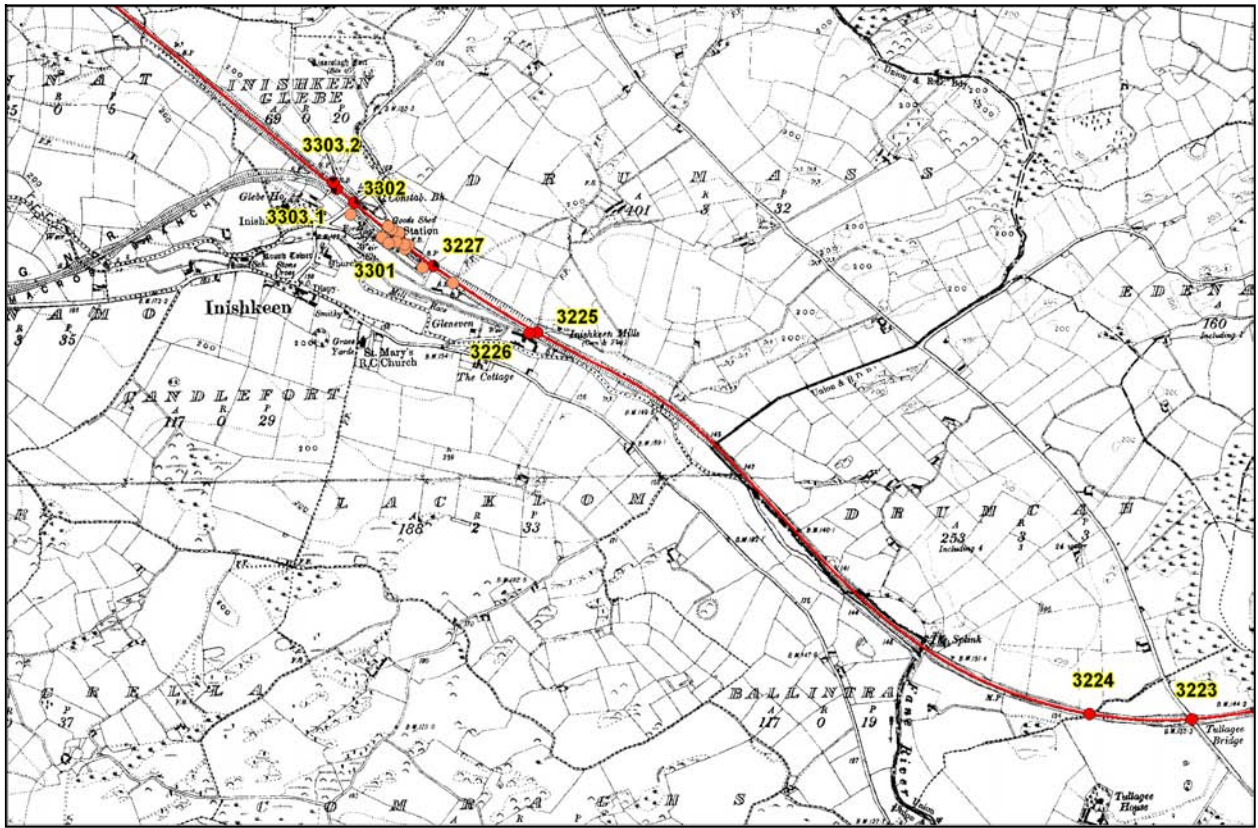




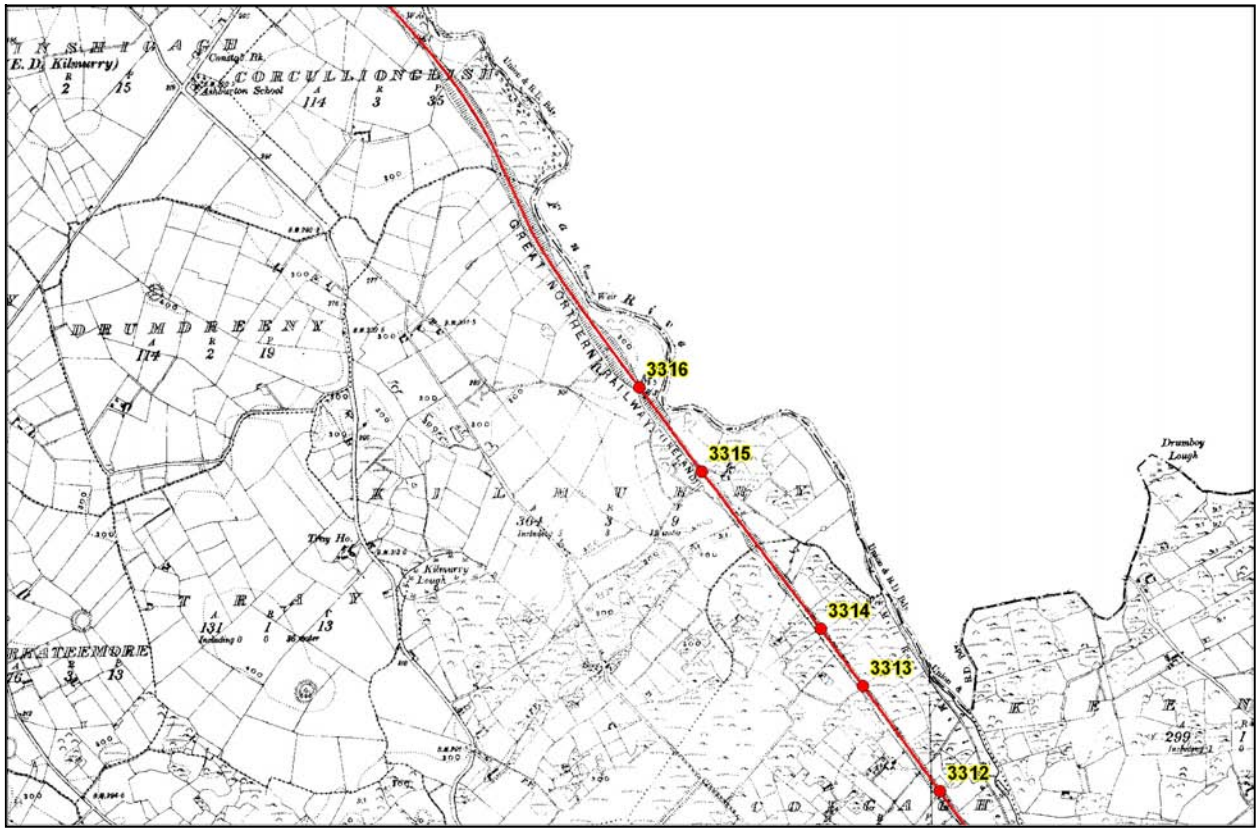
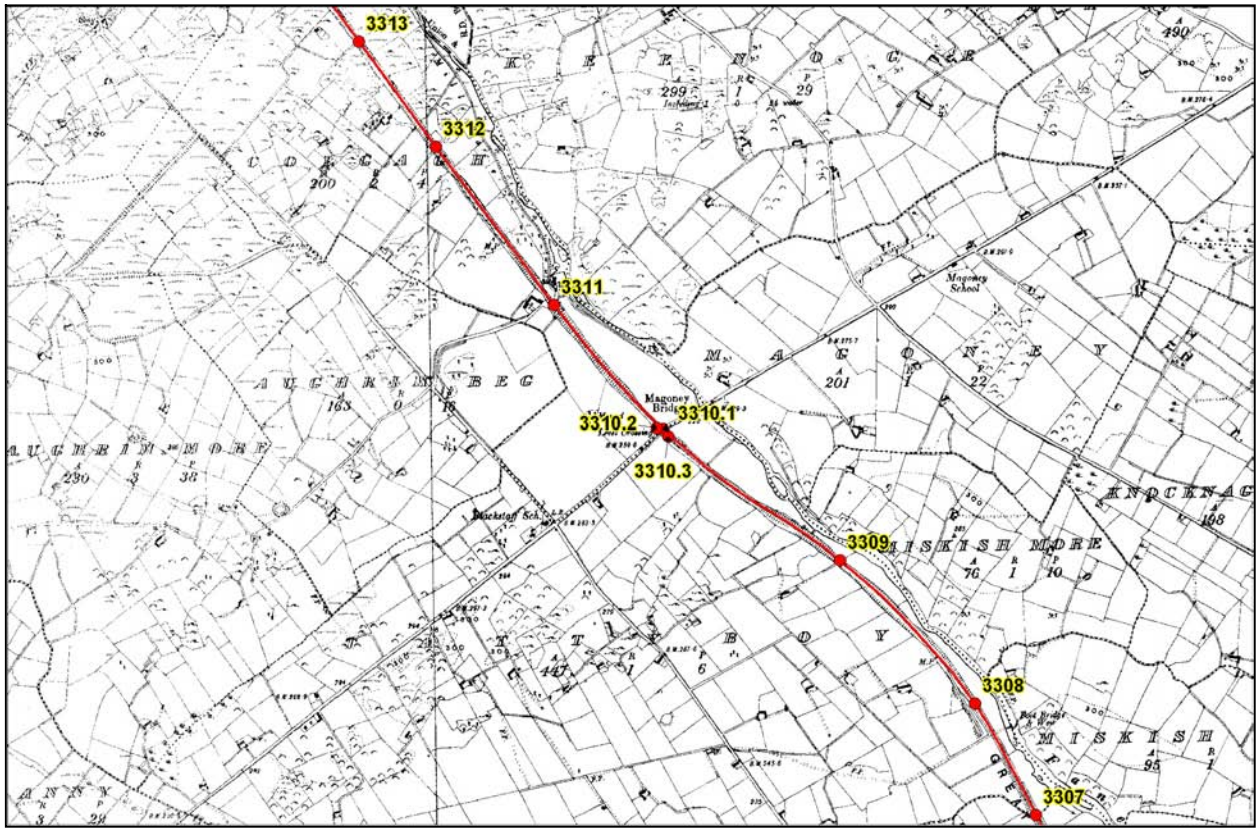
Site maps 3101 - 3209



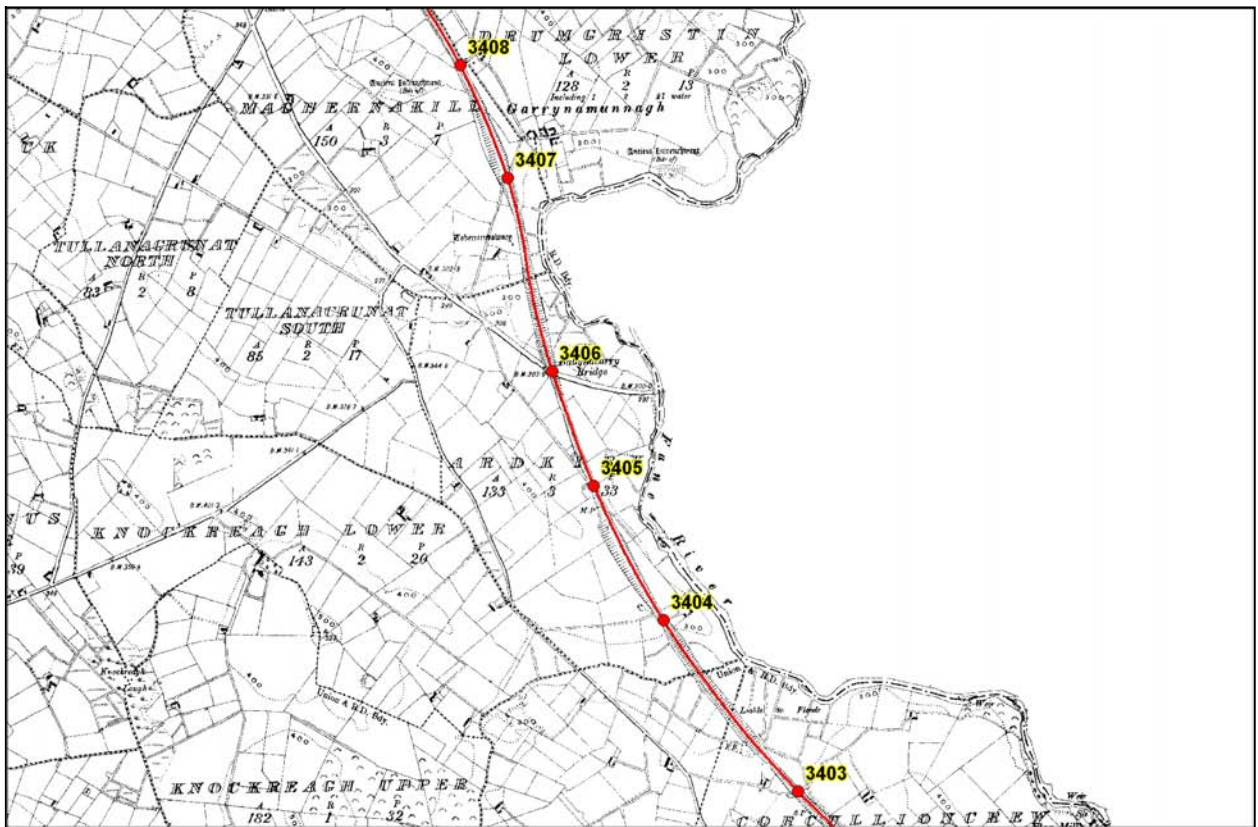
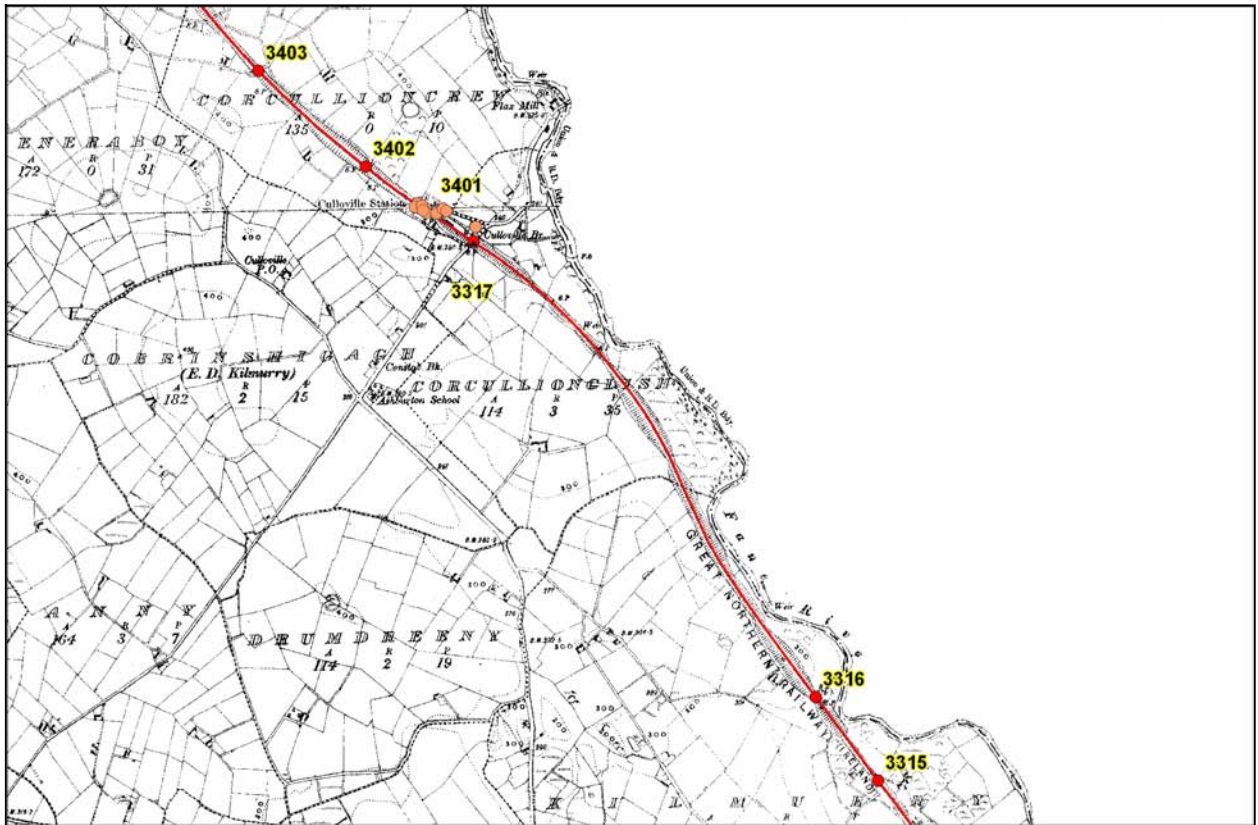
Site maps 3208 - 3224



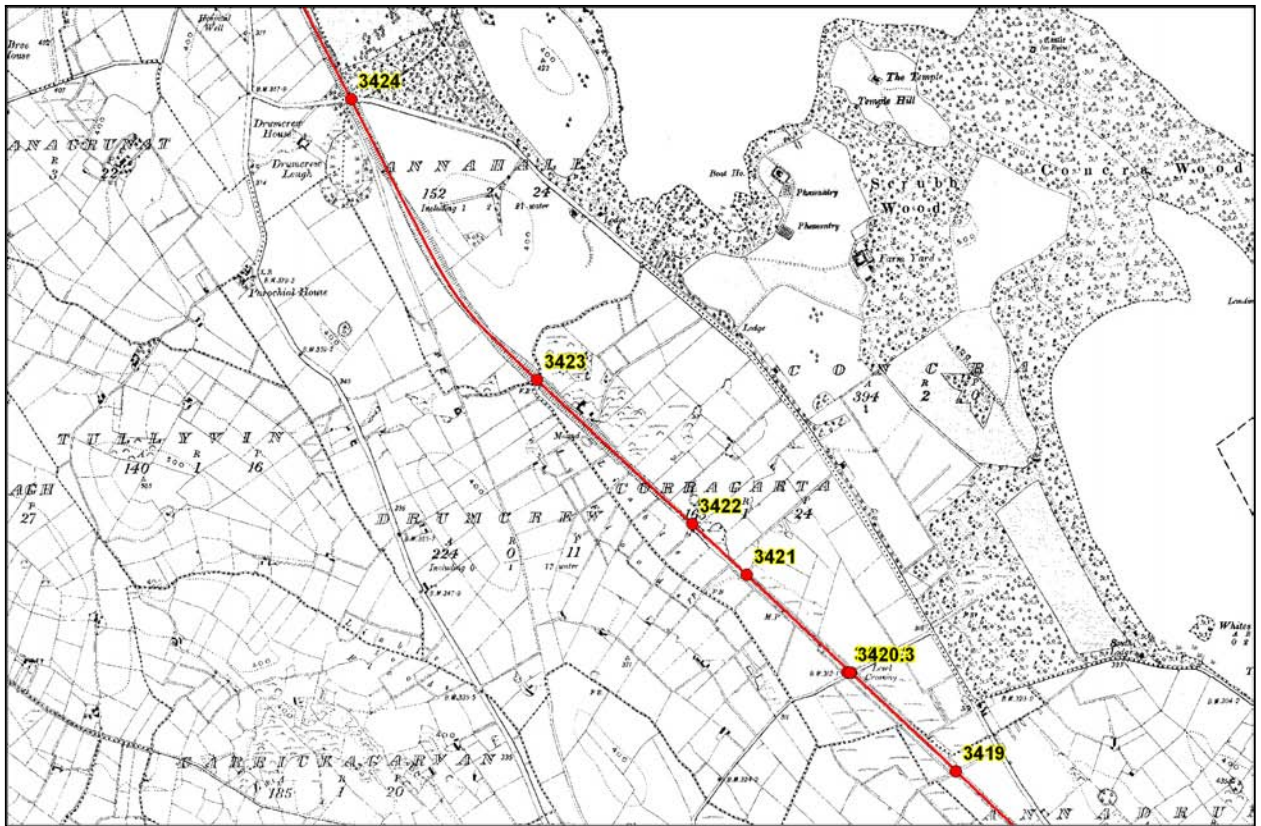
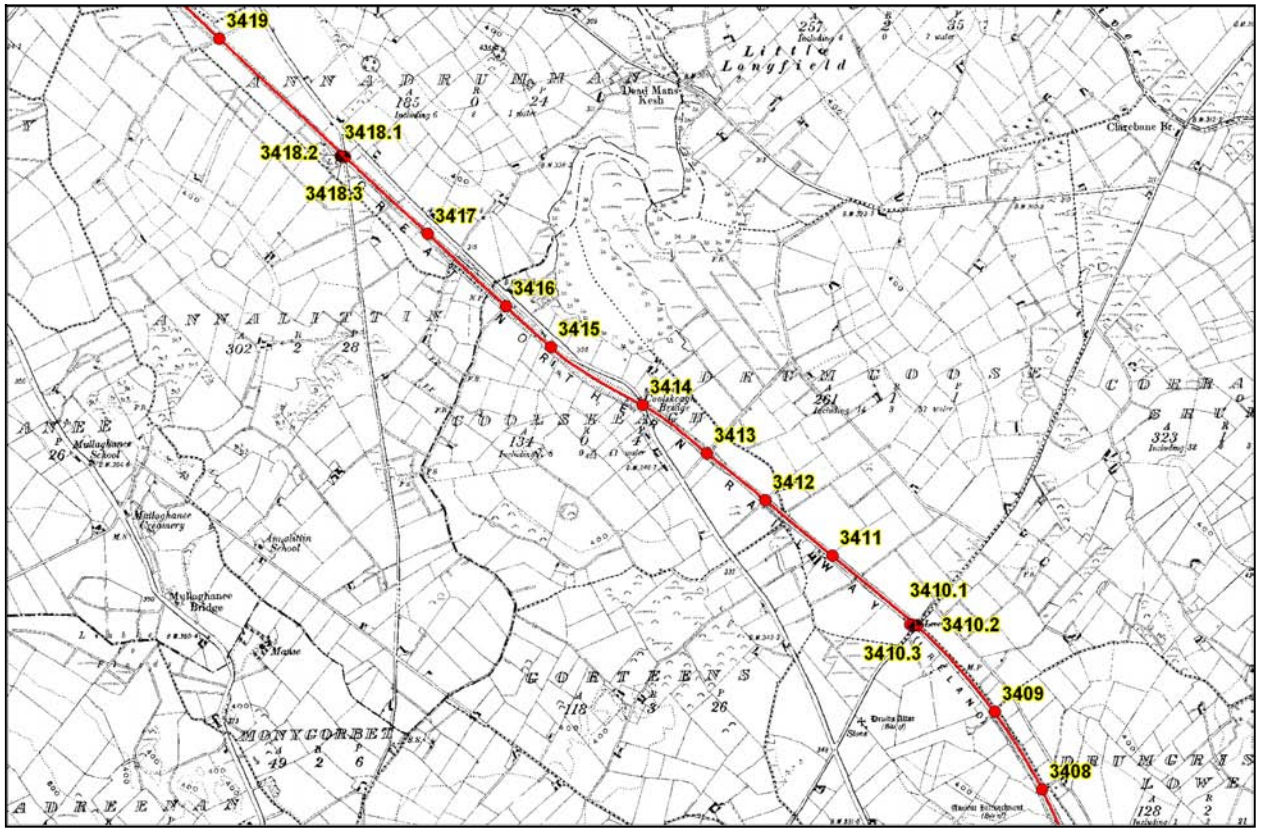
Site maps 3223 - 3308



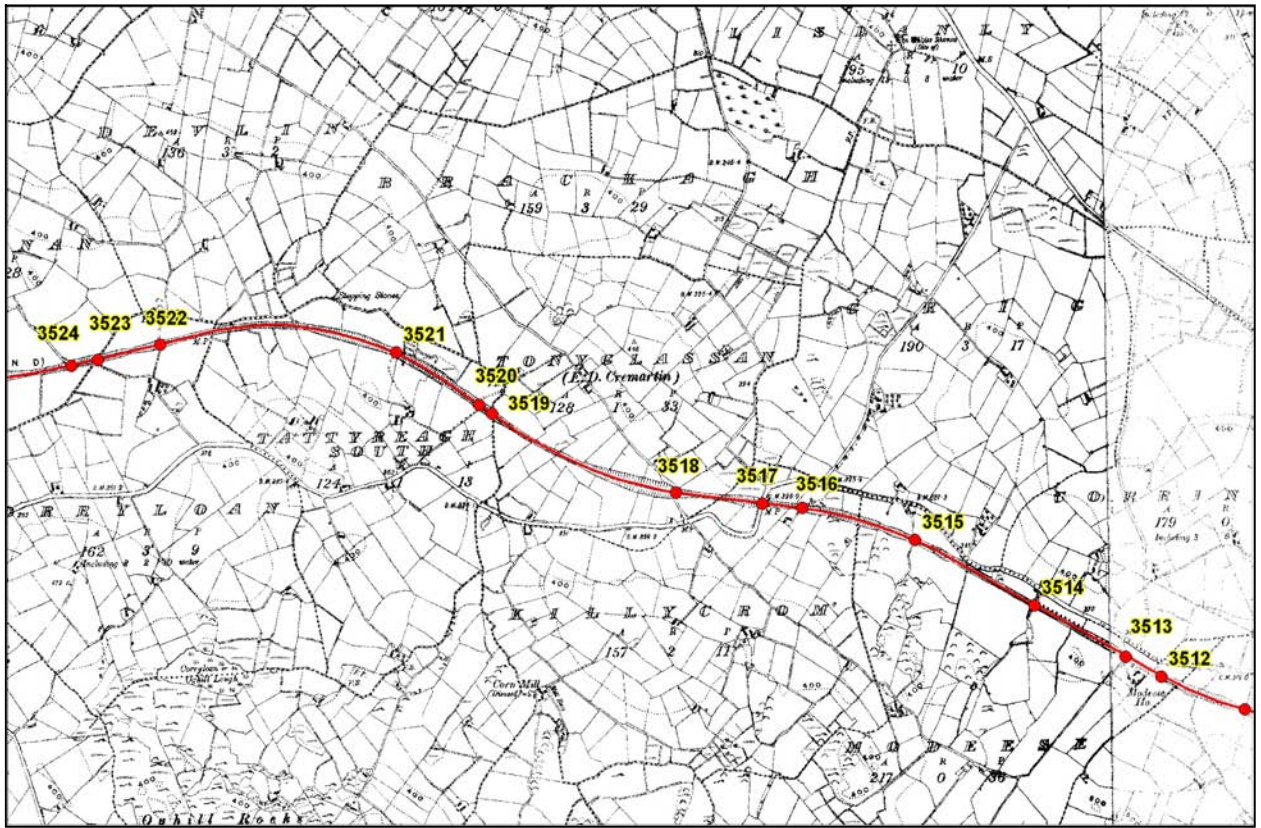
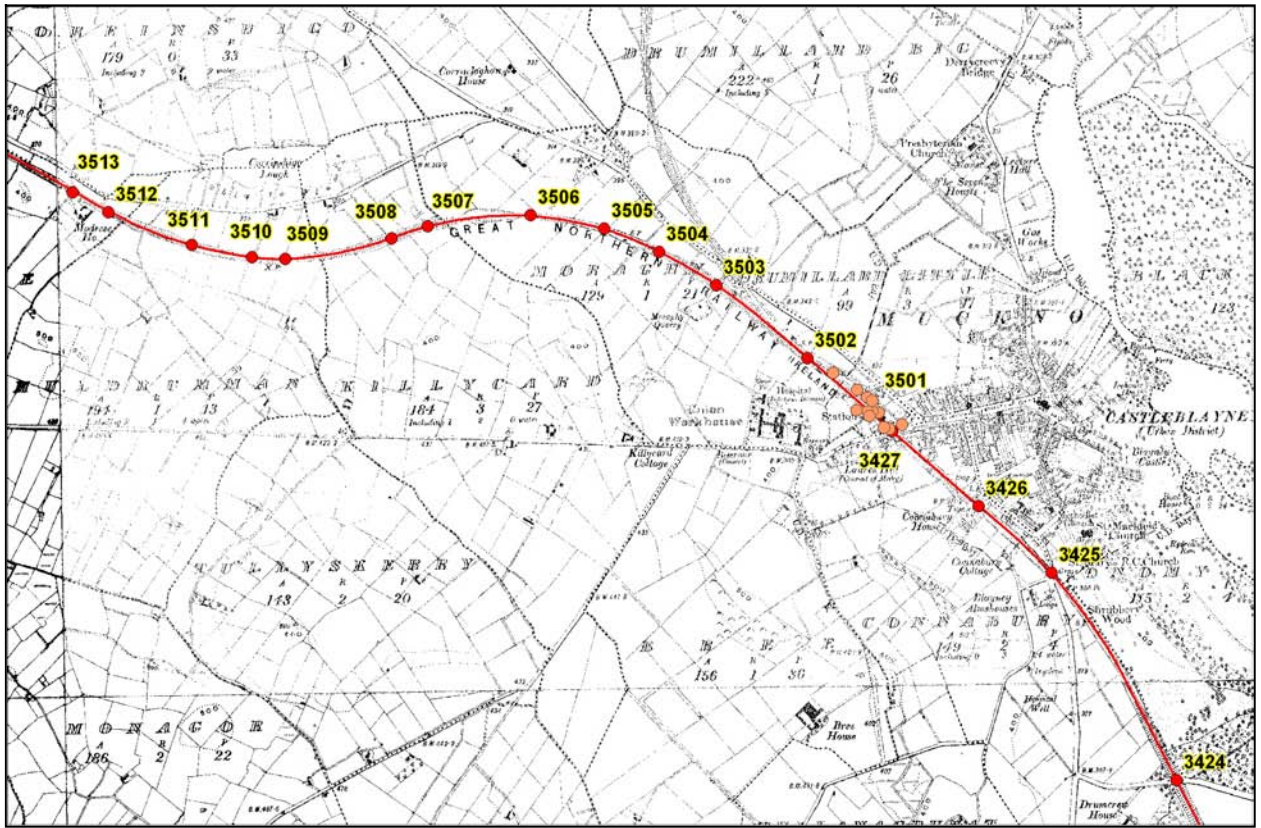
Site maps 3307 - 3316



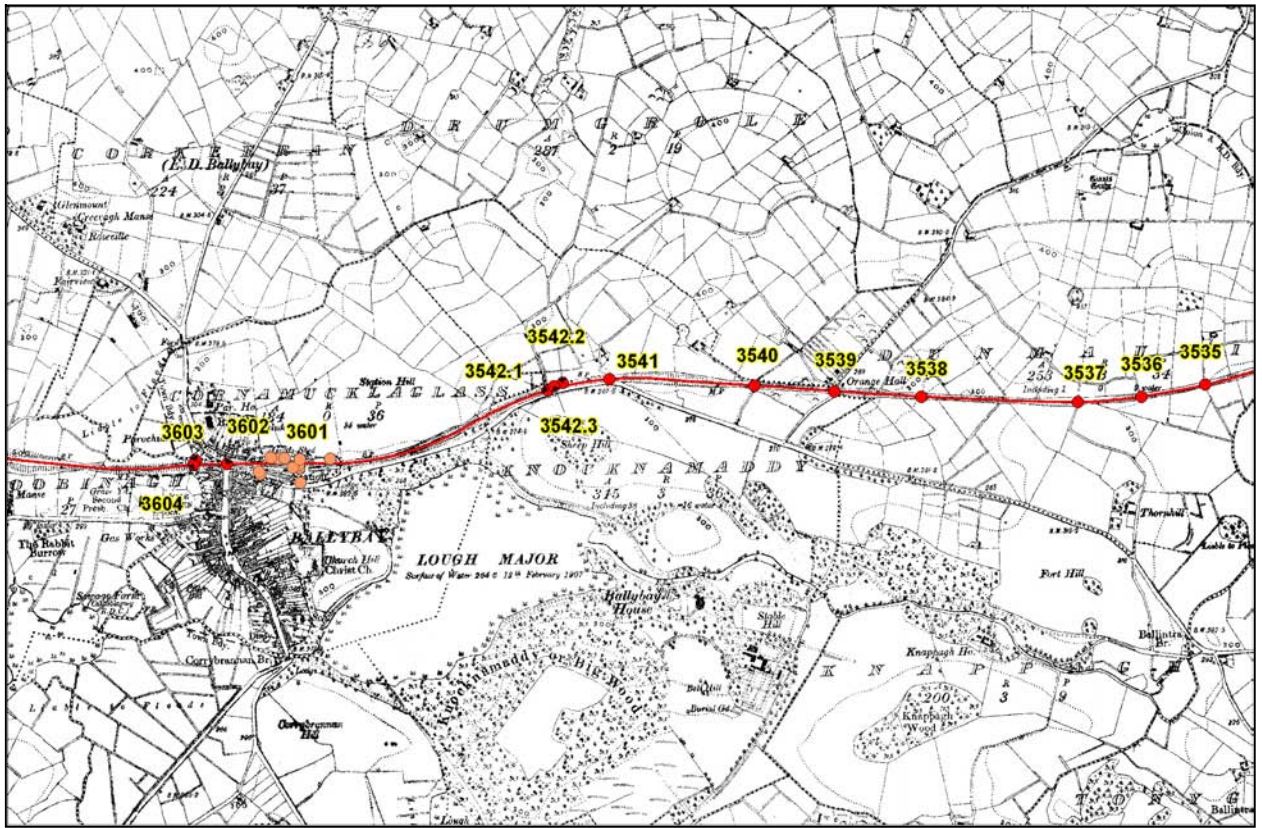
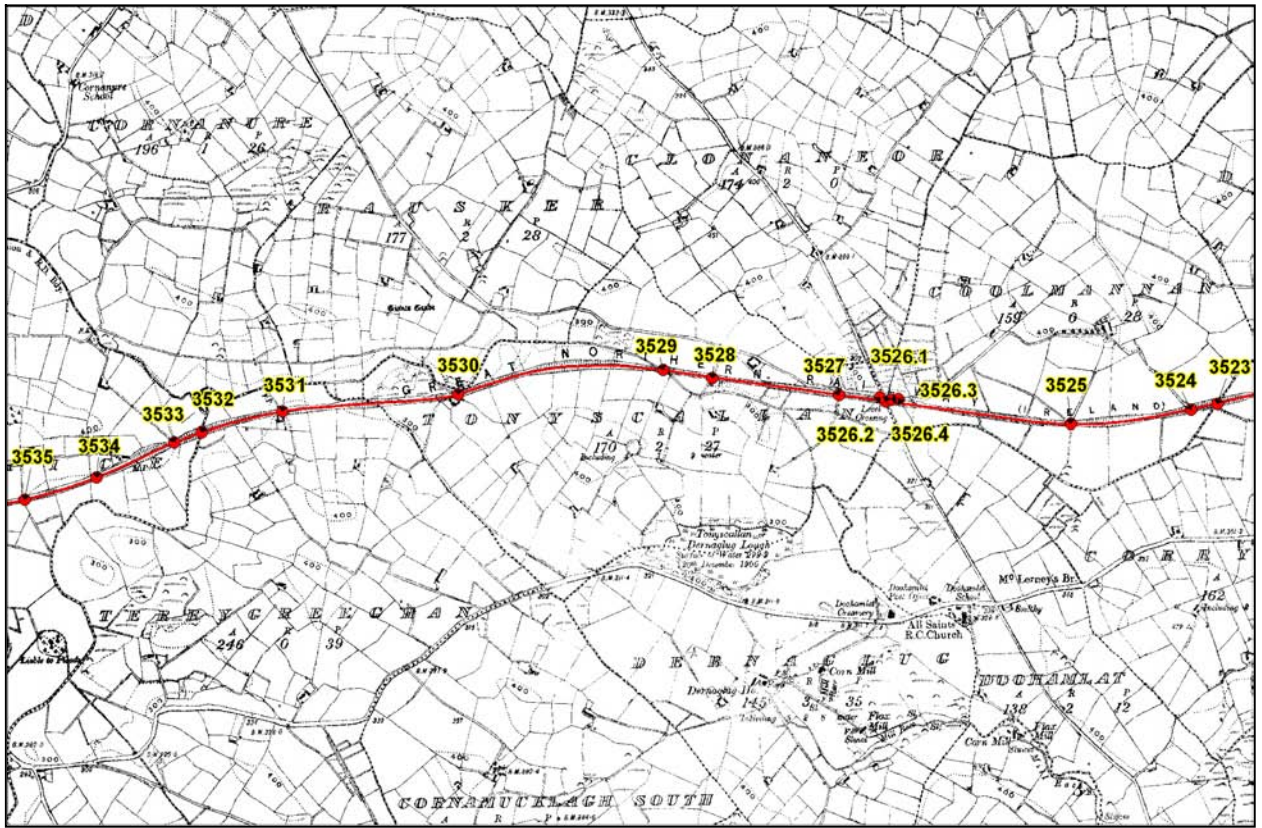
Site maps 3315 - 3408



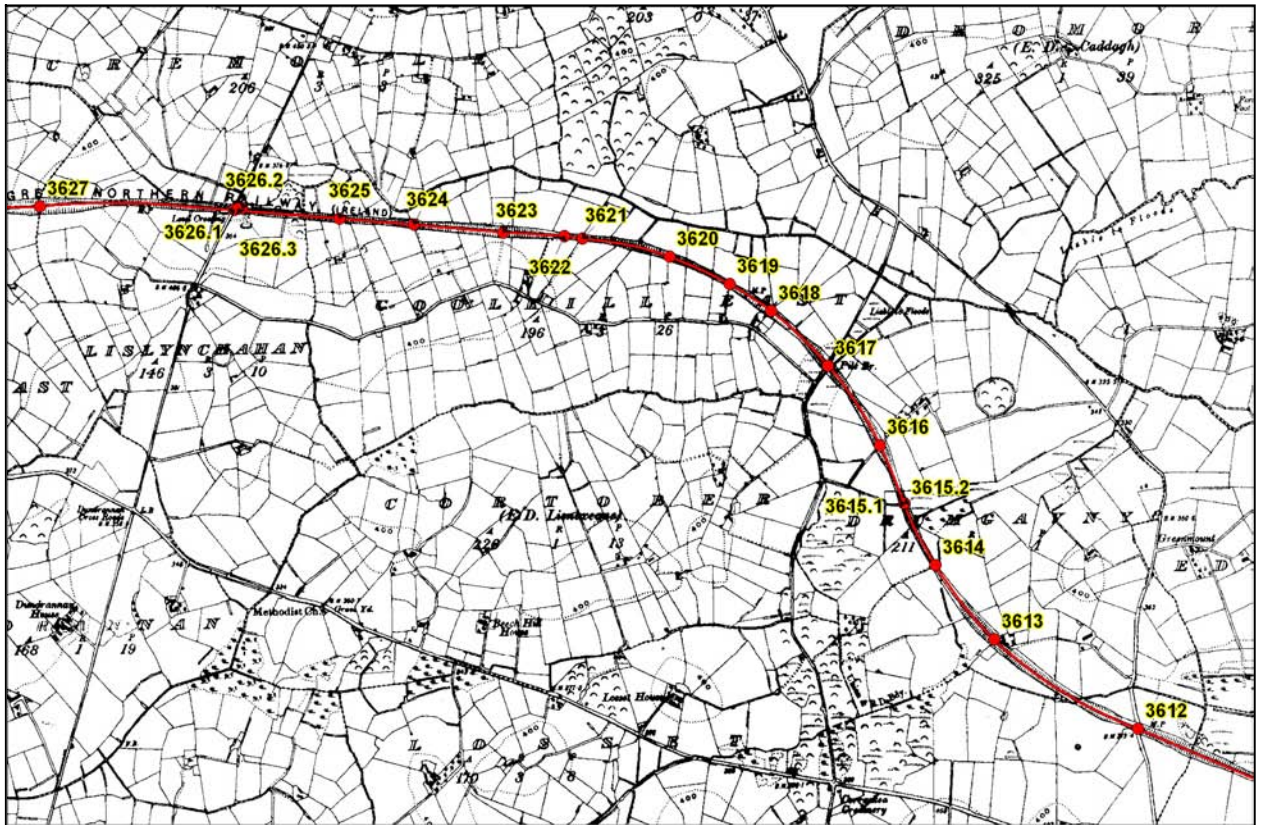
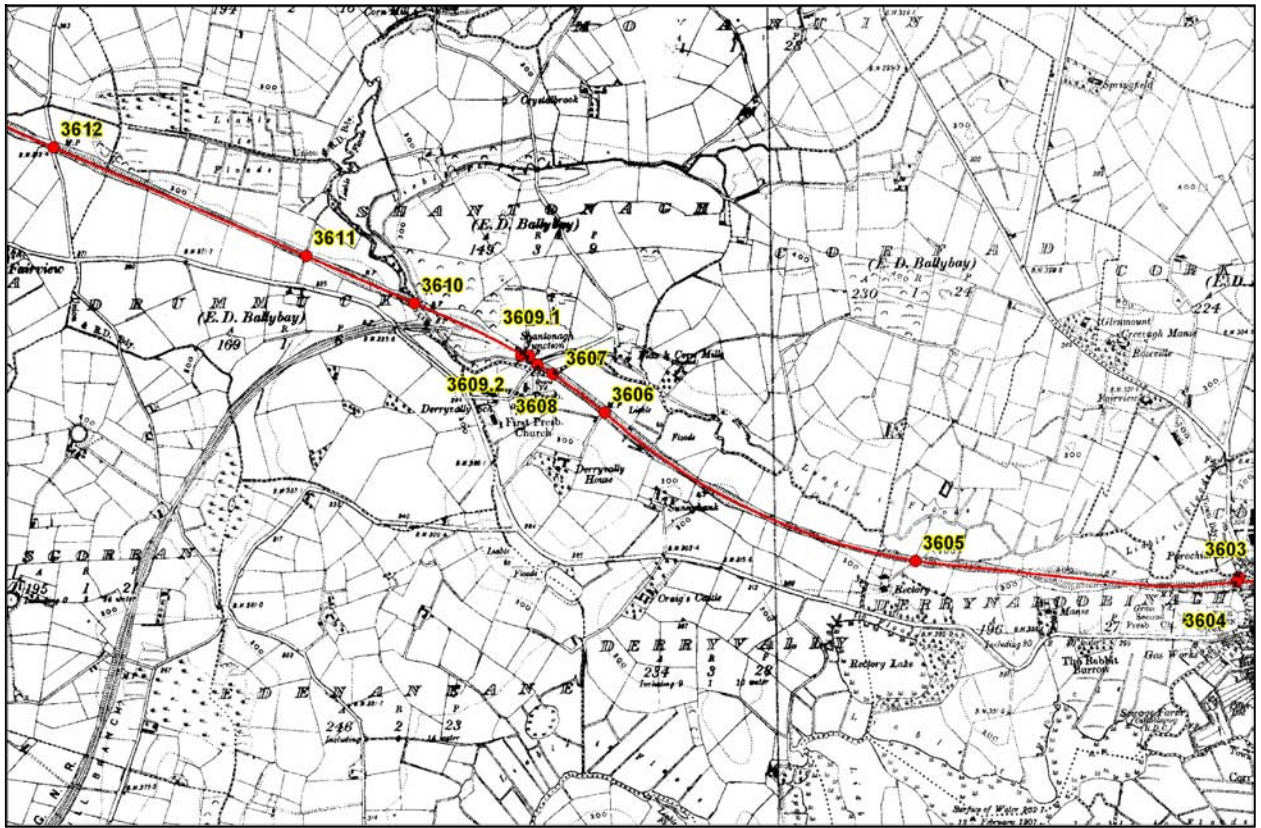
Site maps 3408 - 3424



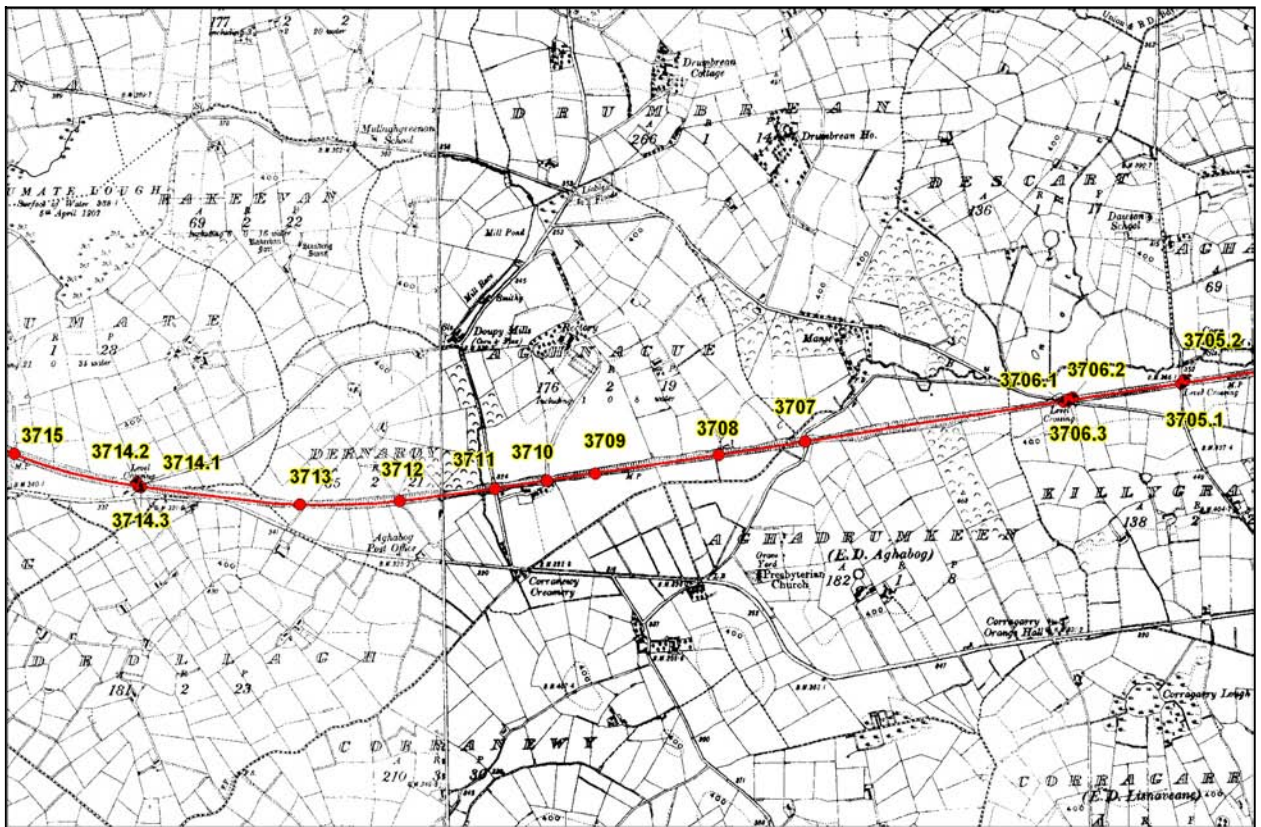
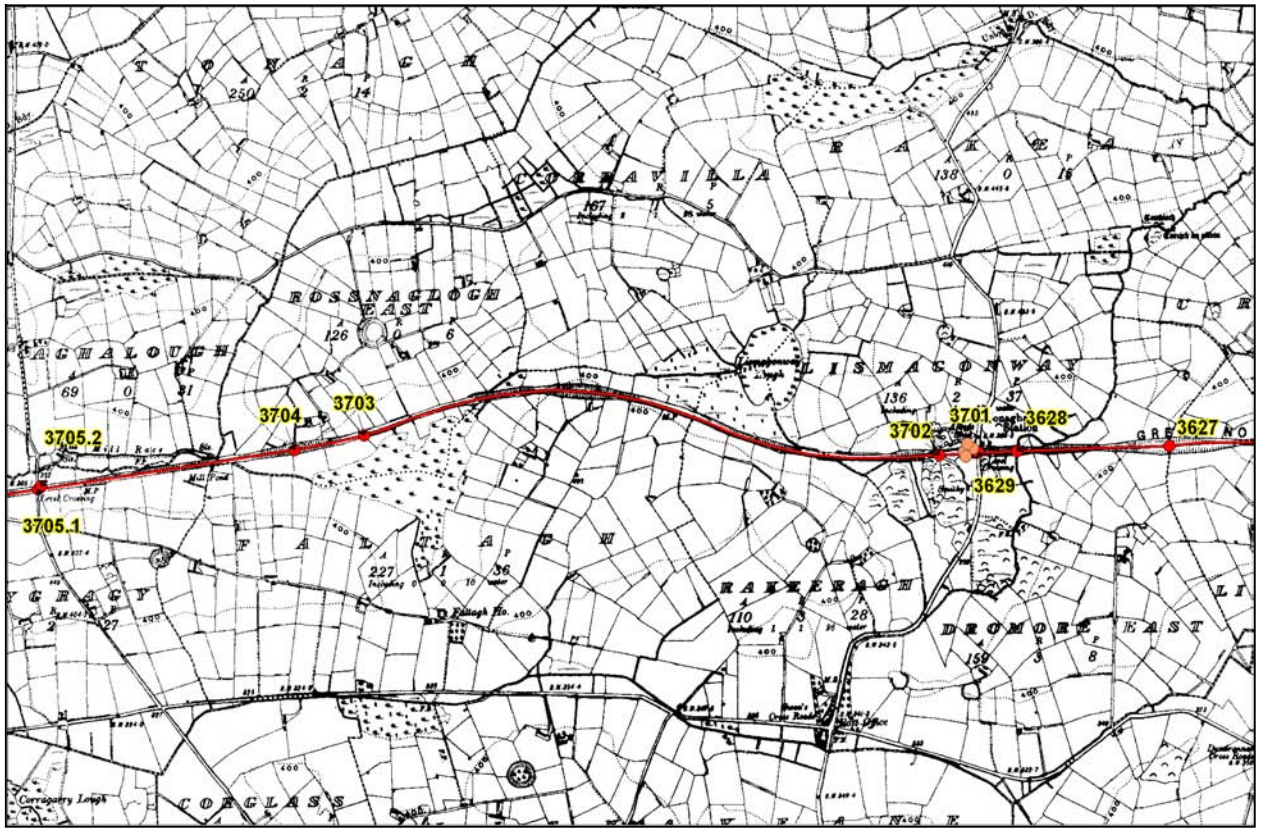
Site maps 3424 - 3524



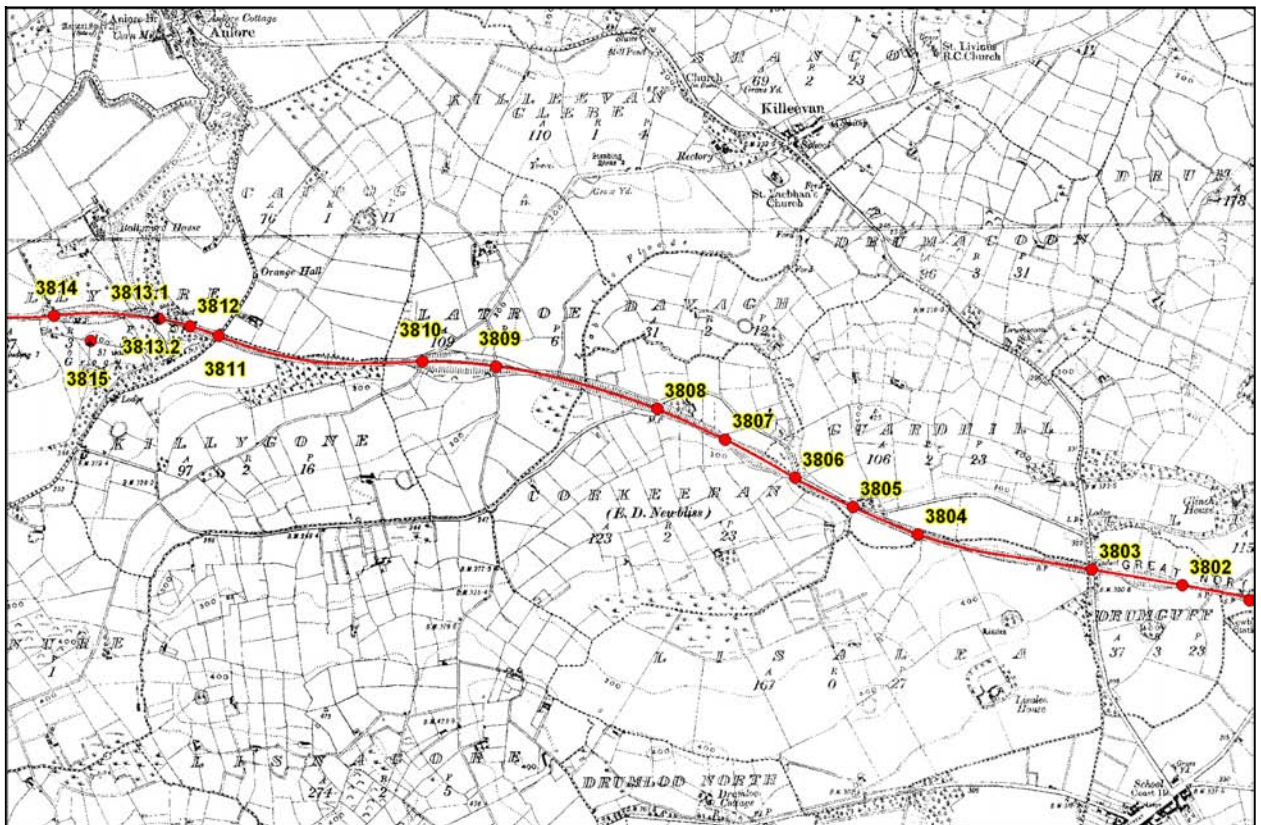
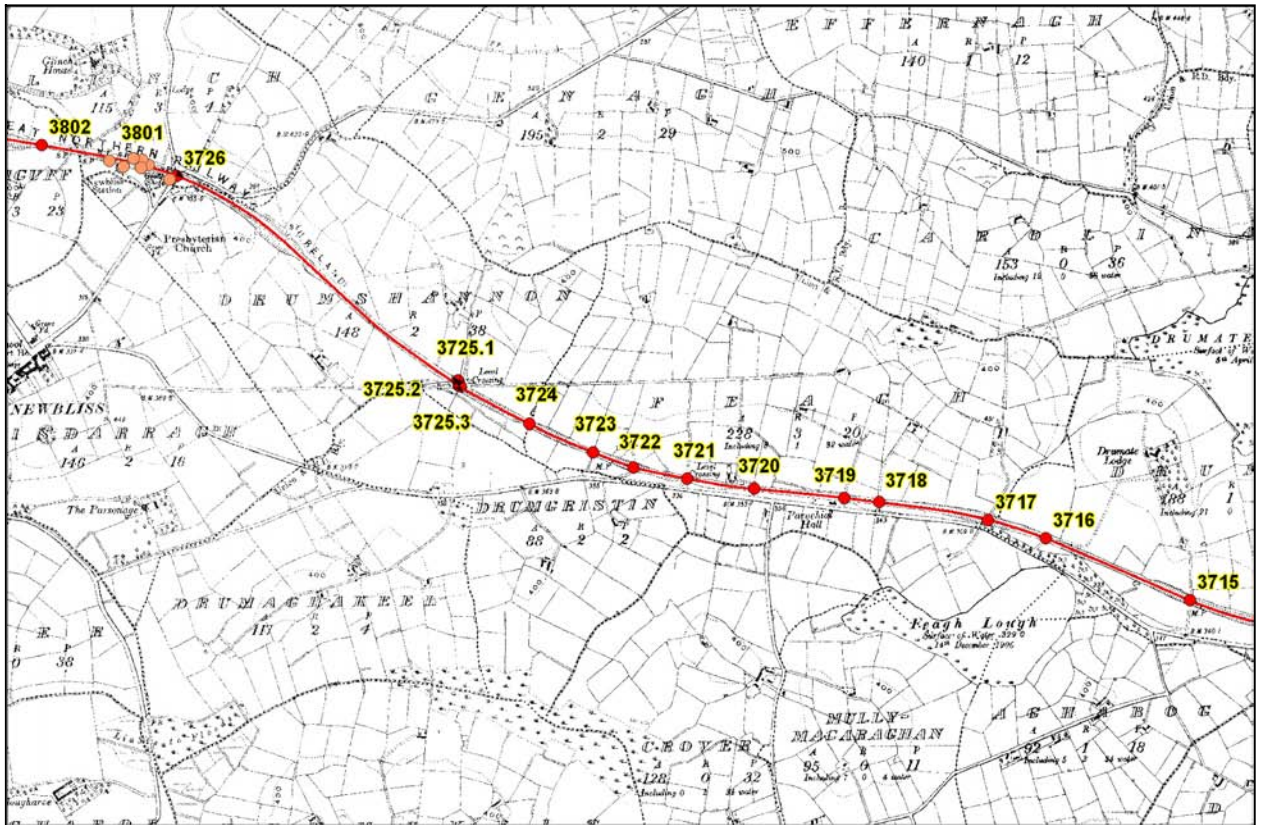
Site maps 3523 - 3604



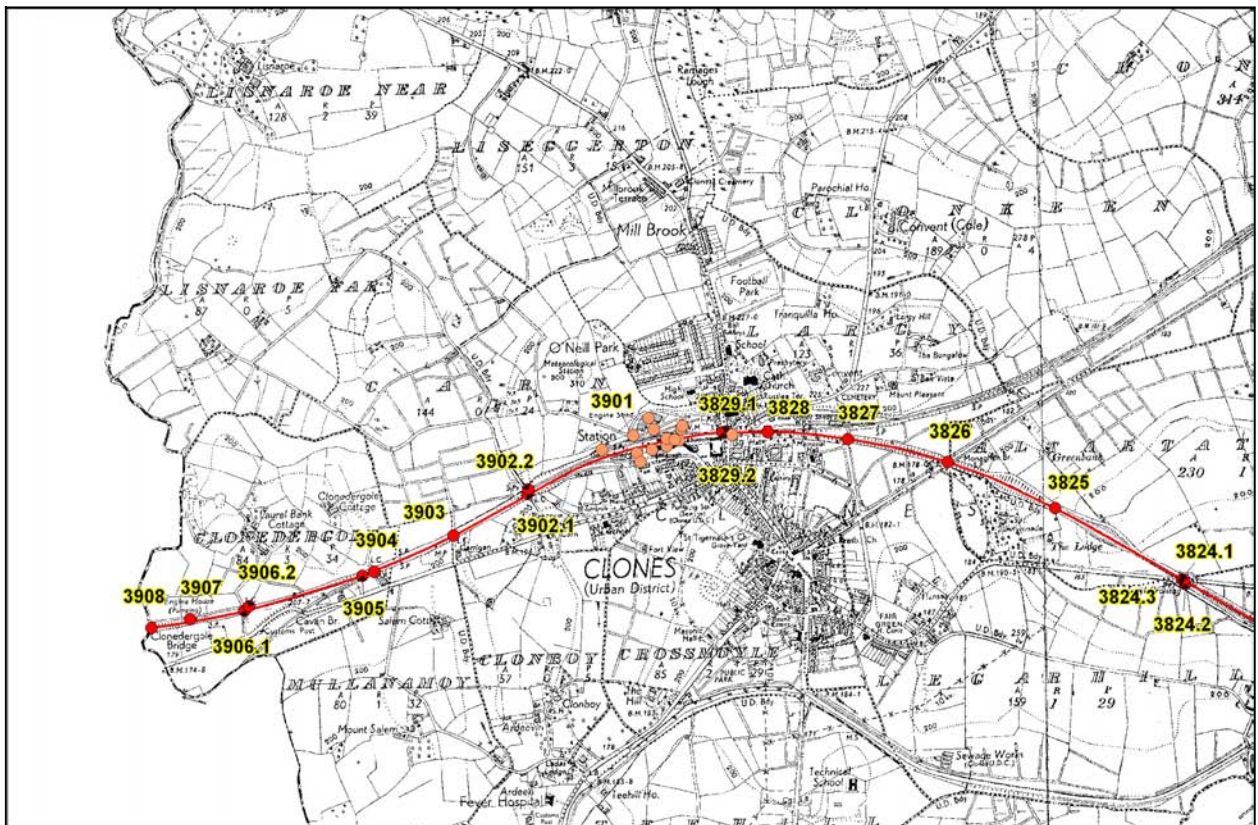
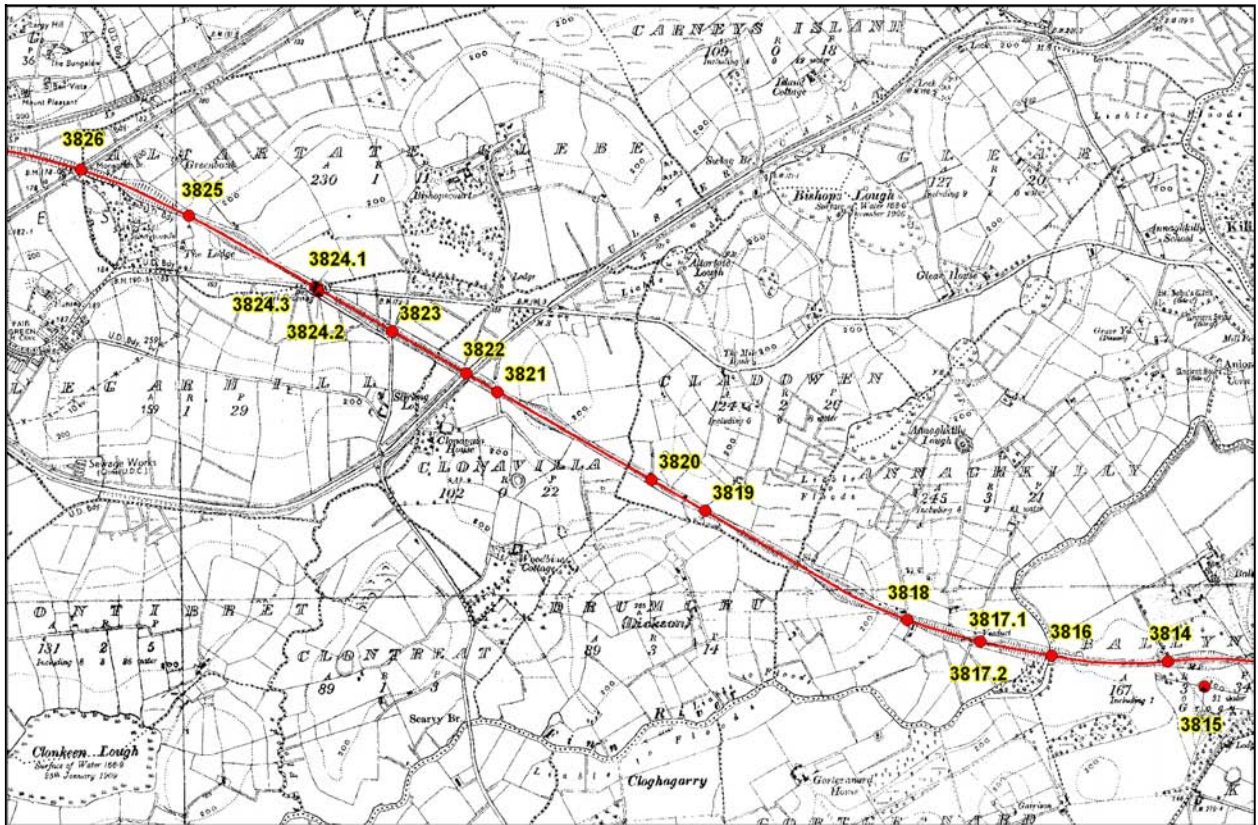
Site maps 3603 - 3627



Site maps 3627 - 3715



Site maps 3715 - 3815



Site maps 3814 - 3908

History Opened in May 1873 to link with the Dundalk & Greenore Railway. Passenger traffic ceased in December 1951, and goods traffic in 1955.

Component	03101	01	Type	Junction		
County	Louth		OS 6-inch map	LH007	Grid	305815 307441
Surveyed?	Y		Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	Nothing survives of this head-on junction with Dundalk-Greenore line. This section of line has been redeveloped as a road.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History

Line opened by the Dundalk & Enniskillen Railway in February 1849. This building is depicted on the 1863 OS map, together with a turntable just to its south. It was used by the Dundalk & Enniskillen Railway/ Irish North Western Railway as an engine works until the opening of the GNR(I) Works on the Ardee Road c.1880. The complex became the main Dundalk goods depot. Closed by Iarnród Éireann in 1995. The water tank (component 2) is not shown on the 1863 OS map. The goods shed (3) is cited as a goods store on this map, but the saw mill (4) is not shown. The engine shed (6 and 7) are cited as an engine house and carriage shed respectively on the map. The former was removed sometime after 1867. The signal box (8) dates from the period 1877-1903 and is associated with the connecting link to the Dundalk & Greenore Railway, opened in May 1873. Passenger traffic on this line ceased in December 1951, and goods traffic in 1955.

03102_01 Derek Henderson Source C. Friel Collection.

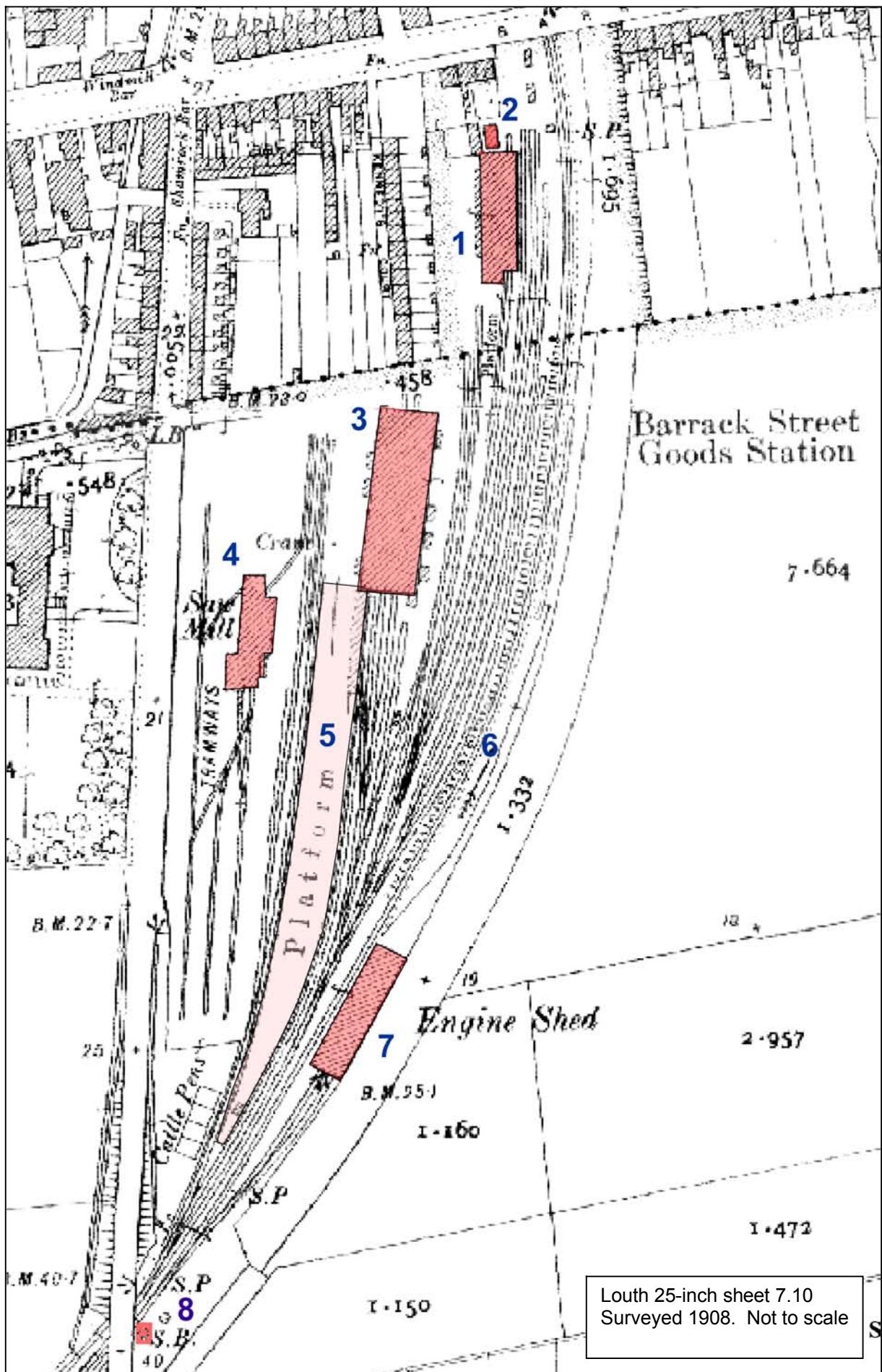
Dundalk Barrack Street goods yard as seen from the overbridge on St Alponus Road.



03102_02 A.E. Bennett 20/06/1958 Source Ferris 2003, p.93.

SG3 class 0-6-0 No.40 shunting at Barrack Street Depot.

Component	03102 01	Type	Engine works			
County	Louth	OS 6-inch map	LH007	Grid	305791	307404
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Offices	
Description	<p>This building is aligned north-south at the north end of the site. Its shell has recently been extensively refurbished as offices. Its gabled artificial slate roof is completely new. The walls are of random rubble masonry; their upper third is a new build as well, added to create a first floor internally. Original brick quoins survive at NE and SE.</p> <p>The east elevation has seven brick-trimmed semicircular openings, all now partly infilled and glazed. New windows run just below eaves level in the raised first floor. The heightening of the building is particularly evident in the north gable. There is a rusticated and margined limestone quoin at its right (NW) end.</p> <p>The west elevation is also seven openings wide. There is a rusticated and margined limestone quoin at its right (SW) end. These openings are trimmed with brick, but have segmental heads. Alternate openings are slightly higher. They appear to have been doorways originally, but all but one has been partly infilled and glazed to create windows. There are also windows along the first floor.</p> <p>A steel and glass link block to an enclosed modern staircase abuts the middle of the</p>					



Louth 25-inch sheet 7.10
 Surveyed 1908. Not to scale

south gable. The exposed wall at left contains a window and door opening, both brick trimmed. The exposed right-hand end contains a larger brick-trimmed semicircular loco entrance. The abutted section of wall has been plastered and has openings to both floors.

The small single-storey building immediately SW of it is modern.

Interest	Architectural; Historical; Group
Evaluation	Although considerably altered, some of this building's original character has been retained. This, together with its historical association with the railway and proximity to the water tank at its north end, make it of local industrial heritage significance.
Significance	Local

Component	03102 02	Type	Water tank			
County	Louth	OS 6-inch map	LH007	Grid	305788	307428
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	Comprises a cast-iron open tank over a single-storey rubble masonry base. There are brick quoins to the latter and also brick trimmed openings, all with segmental heads. There is a doorway on the east elevation, flanked by a window opening to each side (with replacement frames). The south elevation is blank. The west elevation has an infilled doorway. The north elevation could not be accessed at the time of survey, nor the interior.					
Interest	Architectural; Historical; Group					
Evaluation	This is the most complete surviving structure in the entire depot. Its architectural interest, railway associations, and juxtaposition with the former engine shed make it of local industrial heritage significance.					
Significance	Local					

Component	03102 03	Type	Goods shed			
County	Louth	OS 6-inch map	LH007	Grid	305762	307308
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Louth County Council headquarters now occupy site of building.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03102 04	Type	Saw mill		
County	Louth	OS 6-inch map	LH007	Grid	305714 307265
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Manufactured sleepers etc. Nothing remains. The area is now a car park for Louth CC offices.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03102 05	Type	Platform		
County	Louth	OS 6-inch map	LH007	Grid	305740 307217
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Crane at north end. Presumably a goods/animal beach. Nothing survives.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03102 06	Type	Engine shed		
County	Louth	OS 6-inch map	LH007	Grid	305793 307212
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03102 07	Type	Engine shed		
County	Louth	OS 6-inch map	LH007	Grid	305747 307139
Surveyed?	Y	Survey date	30/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03102 08	Type	Signal box		
County	Louth	OS 6-inch map	LH007	Grid	305683 307041
Surveyed?	Y	Survey date	30/11/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	A road now runs along the former line of the railway at this point and no traces of the signal box survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03102_01 FWH 09/07/2007 General view of former engine works from SE.



03102_02 FWH 09/07/2007 Engine works from SW.



03102_03 FWH 30/10/2007 East elevation of engine works, from NE.



03102_04 FWH 30/10/2007 SE end of engine works showing former entrance for locomotives.



03102_05 FWH 30/10/2007 Water tank from SE.



Site 03103

History According to the late Maureen Wilson, this road bridge over the railway was built in 1883.

Component	03103 01	Type	Bridge (road/rail)			
County	Louth	OS 6-inch map	LH007	Grid	305670	307041
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of bridge have been removed. A road now runs along the former course of the railway at this point.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History Date unknown, but probably early/mid 1900s.

Component	03104 01	Type	Siding
County	Louth	OS 6-inch map	LH007 Grid 304774 306516
Surveyed?	Y	Survey date	31/07/2007 Surveyor Fred Hamond
Completeness	No visible remain	Condition	N/A Use N/A
Description	The former siding has been recently redeveloped and no traces of the line survive. The former pork factory was in process of being demolished at the time of survey.		
Interest	None		
Evaluation	No industrial heritage significance.		
Significance	Record only		

Site 03105

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Passenger traffic ceased in December 1951, and goods traffic in 1995. The footbridge (component 2) is cited as a wooden footbridge on the 1863 OS six-inch map. It was replaced by a metal footbridge in the period 1877-1903.

Component	03105 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH007	Grid	304712	306532
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Although this crossing is still a public right of way, nothing survives of the actual road crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03105 02	Type	Bridge (foot/rail)			
County	Louth	OS 6-inch map	LH007	Grid	304718	306533
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this footbridge over the railway survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site**03106****Dundalk East Cabin (DKE); East Junction**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Line between East and West junctions closed by GNR(I) in December 1951. The signal cabin (component 2) dates from the period 1877-1903.

Component	03106 01	Type	Junction		
County	Louth	OS 6-inch map	LH007	Grid	304627 306514
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of this junction survive just east of Dublin Road Bridge.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03106 02	Type	Signal box		
County	Louth	OS 6-inch map	LH007	Grid	304516 306487
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of this signal box survive just west of Dublin Road Bridge.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03106_01 FWH

31/07/2007 Looking NE between Dundalk-Enniskillen line and main Dublin-Belfast line.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. According to Mills (1903), the bridge deck was strengthened in the period 1877-1903. Line between East and West junctions closed by GNR(I) in December 1951. The present deck is a modern replacement.

Component	03107 01	Type	Bridge (road/rail)			
County	Louth	OS 6-inch map	LH007	Grid	304581	306505
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Road bridge	
Description	A slightly skew single-span beam bridge carries a road over the former double-line former railway. The abutments, which are 12.10m deep, are of rusticated masonry blocks with tooled quoins. The original deck (probably a metal girder span) has been replaced with pre-cast reinforced-concrete beams. The span is 9.09m, measured orthogonally, and 9.36m on the skew. The parapet comprises vertical steel railings which terminate in concrete block piers. There are split random rubble wing walls to each side. At the top of the NE quoin are the remnants of telegraph wire insulators.					
Interest	None					
Evaluation	The replacement deck has diminished this bridge's architectural character and it is now of no special industrial heritage significance.					
Significance	Record only					

03107_01 FWH 31/07/2007 East elevation of bridge.



03107_02 FWH

31/07/2007 West elevation of bridge.



History

Line opened by the Dundalk & Enniskillen Railway in February 1849. The signal box (component 2) replaced a watch house (captioned on 2nd edition OS map) and dates from the period 1877-1903. Line between East and West junctions closed by GNR(I) in December 1951. Square Crossing subsequently lifted.

03108_01 Drew Donaldson 1958 Source C. Friel Collection: DDN9.

Dundalk: The signal cabin at the Square Crossing, 1958. By now the INWR tracks across the main line have been lifted. Here V class 4-4-0 No 86 "Peregrine" approaches with 2.30pm from Dublin to Belfast. In the right foreground, the DBJR station platform is still in situ.



03108_02 F. Duffner 21/04/1945 Source Duffner R516.

Looking north at the Square Crossing with General Store behind, with the line from Greenore crossing from right. The footpath running left is the former D&BJR station platform which runs to the Ardee Road overbridge.



03108_03 Unknown

Source Wilson M., Ross N, and Power P.F., Dundalk: Images and Impressions, p.56 (Dundalk, 1957).

The Square Crossing.



Greenore train traversing the Square Crossing.



Component	03108 01	Type	Level crossing (rail-rail)		
County	Louth	OS 6-inch map	LH007	Grid	304170 306417
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of this crossing over the main Dublin-Belfast line survive. The latter has been relaid.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03108 02	Type	Signal box		
County	Louth	OS 6-inch map	LH007	Grid	304178 306423
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing of this former signal cabin survives.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03108_01 FWH

31/07/2007 View of former Square Crossing, from north.



Site 03109

History Line opened by the Dundalk & Enniskillen Railway in February 1849. The original span was probably replaced with the present lattice girders in the early 1900s (but post 1903 as not cited in Mills). The line between Dundalk East and West junctions closed by GNR(I) in December 1951.

Component	03109 01	Type	Bridge (road/rail)			
County	Louth	OS 6-inch map	LH007	Grid	304054	306391
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Road bridge	
Description	<p>A highly skewed metal beam bridge carries a road (and footpath) over the former double-line track. The abutments are of rock-faced stone blocks, laid to courses and embellished with margined quoins; they are set 9.14m apart (measured orthogonally). The span comprises five riveted metal I beams with smaller lattice girders laid between their bottom flanges and supporting a pre-cast reinforced-concrete slab deck. The principal beams have an overall length of 21.76m and cross section of 76cm by c.45cm. Each beam is supported at centre by a riveted metal column footed in concrete. Metal brackets are affixed to the outer beams and support the parapets. The latter are 8.82m apart and of riveted metal plates. Decorative cast-metal plates have been affixed to the top of each parapet; modern steel strengthening pieces have been added to their back faces. The parapets terminate in ashlar piers. There are angled squared random rubble wing walls at each end.</p> <p>At the NE end of the bridge is a defunct telegraph pole with multiple wire insulators. A pipe is carried underneath the north end of the deck. Metal palisade fence at each end of the span now preclude entry to its underside.</p>					
Interest	Architectural; Historical; Technical; Rarity					
Evaluation	This bridge is the only surviving example of its type on the Co Louth section of the former Dundalk-Enniskillen line, albeit possibly a later replacement. Its masonry abutments are also of architectural note. It also has a historical association with the railway. Overall, it is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03109_01 FWH 03/10/2007 South elevation, from SE.



03109_02 FWH 03/10/2007 Underside of deck from SE.



03109_03 FWH 03/10/2007 Underside of deck from SW.



03109_04 FWH 03/10/2007 SE quoin and parapet detailing.



03109_05 FWH

31/07/2007 Parapet detailing at NE end of bridge.



Site

03201

Dundalk Junction Station (DKJ)

History

Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

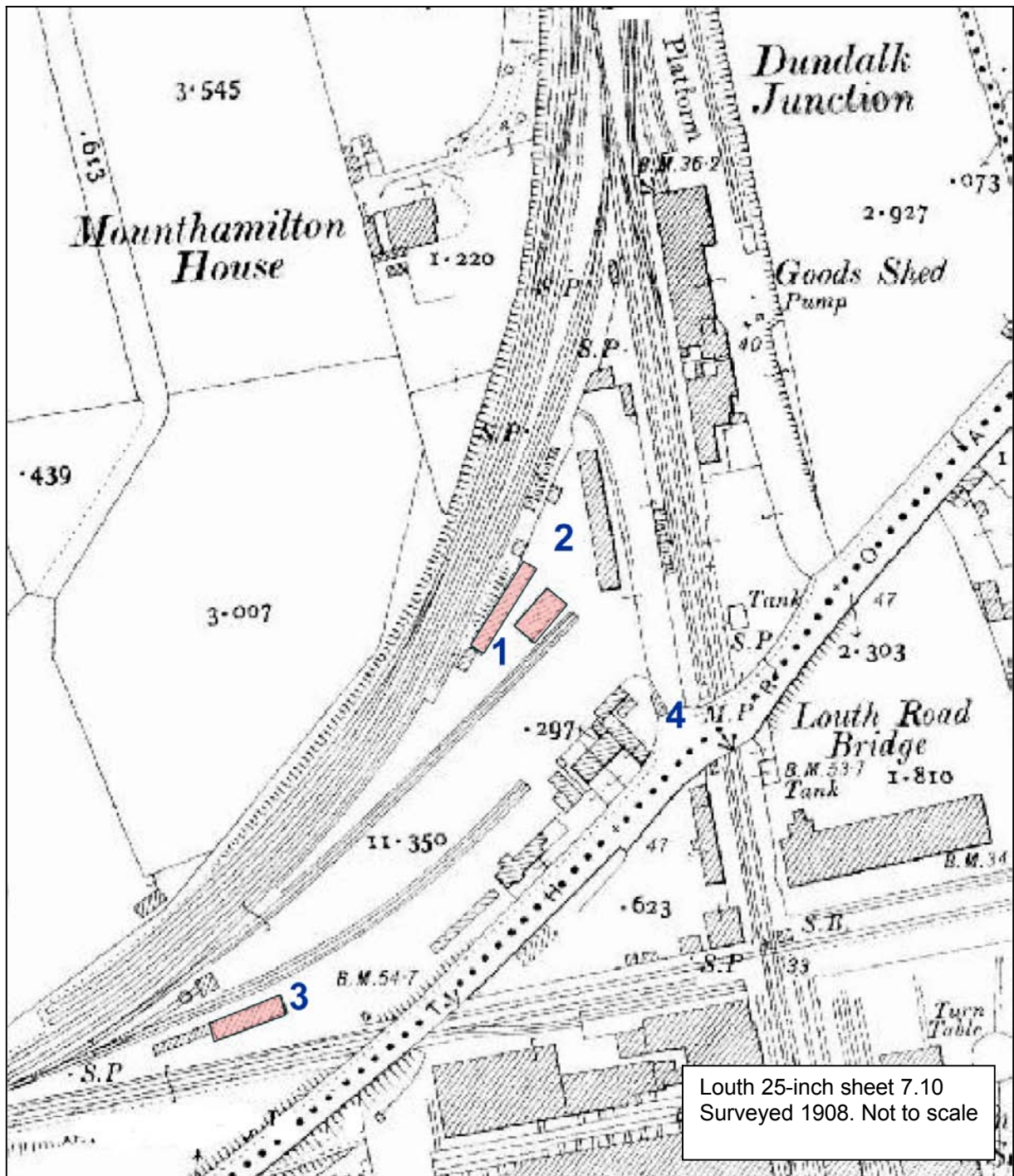
03201_01 Unknown

Source Wilson M., Ross N, and Power P.F., Dundalk: Images and Impressions, p.59 (Dundalk, 1997). Original in Murray, 1944, plate 55.

Dundalk Junction Station.



Component	03201	01	Type	Station building		
County	Louth		OS 6-inch map	LH007	Grid	304085 306526
Surveyed?	Y		Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	Some remains		Condition	Poor	Use	Disused
Description	It was not possible to enter this complex, thereby precluding detailed inspection. The entire area has been redeveloped as Dundalk Freight Yard (closed since June 2007). The ruinous remains of a single-storey building just north of the entrance from the Ardee Road may have been associated with the station building. It is of random rubble, with a brick chimney. On its south gable are three semicircular-headed window openings trimmed with brick.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					



Component	03201 02	Type	Platform		
County	Louth	OS 6-inch map	LH007	Grid	304106 306554
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing appears to survive of the platform, all presumably having been cleared when the site was redeveloped as a freight depot. It linked up with the 'down' platform on the main Dublin-Belfast line.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03201 03	Type	Railway-related building		
County	Louth	OS 6-inch map	LH007	Grid	304005 306398
Surveyed?	N				
Description	As the site could not be accessed, this building could not be inspected. The NIAH survey notes a six-bay, single-storey shed.				
Interest	Architectural; Historical				
Evaluation	The NIAH image indicates that this building is probably of local heritage significance on account of its architecture and association with the railway. The NIAH rates it as being of regional merit.				
Significance	Local				
Other dbases	NIAH 13707053				

Component	03201 04	Type	Entrance (station)		
County	Louth	OS 6-inch map	LH007	Grid	304144 306496
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Fair	Use	Disused
Description	This pair of gate pillars immediately west of the road bridge over the Dublin-Belfast line are of ashlar limestone and are capped with moulded oversailing limestone blocks.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03201_01 FWH 03/10/2007 Remains of station building from south.



03201_02 FWH 31/07/2007 Entrance gates from south.



Site**03202****Dundalk West Cabin (DKW); West Junction**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Line between East and West junctions closed by GNR(I) in December 1951. The signal cabin (component 2) dates from the period 1877-1903.

Component	03202 01	Type	Junction		
County	Louth	OS 6-inch map	LH007	Grid	303895 306359
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing of this junction survives.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03202 02	Type	Signal box		
County	Louth	OS 6-inch map	LH007	Grid	303681 306313
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of this signal cabin survive. This area has been redeveloped for housing.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Site 03203

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03203 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH007	Grid	303283	306223
Surveyed?	N					

Site 03204

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03204 01	Type	Bridge (rail/river)			
County	Louth	OS 6-inch map	LH007	Grid	302845	306122
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched bridge carries the former embanked line over a drainage channel and accommodation track. The abutments are of squared random rubble, without embellishment save for a chamfered masonry string course running through the arch and around the quoins at spring level. The arch is of semicircular profile. The soffit is of random rubble and the voussoirs are of finely-dressed and vee-jointed limestone blocks. The spandrels and low parapets are of split random rubble. The latter are coped with chamfered masonry copings. There are similarly detailed angled wings walls to each end. They terminate in square piers with pyramidal caps. The embankment has been entirely removed at the west end of the bridge to reveal three random rubble buttresses. A short section of embankment survives at the east end of the bridge.

Interest Architectural; Historical

Evaluation The removal of the railway embankment makes this bridge an incongruous sight in the middle of the field. Although the absence of the embankment diminishes its contextual value, it does reveal its manner of construction, notably the buttressed ends. It also has an historical association with the railway. Overall, it is of local industrial heritage significance.

Significance Local

03204_01 FWH 31/07/2007 General view of bridge from north.



03204_02 FWH

31/07/2007 Bridge from north.



Site 03205

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03205 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH007	Grid	302391	306018
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	The Dundalk bypass now runs through the site of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03206	01	Type	Bridge (road/rail)		
County	Louth		OS 6-inch map	LH007	Grid	301904 305906
Surveyed?	Y		Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	Complete		Condition	Fair	Use	Road bridge
Description	This bridge carries a road over a single-line cutting, since infilled to such an extent that it is now visible above the top of the arch. The arch has a random rubble soffit and rusticated and margined vee-jointed voussoirs. The spandrels and parapets are of squared random rubble. A chamfered string course runs along the base of the parapets. The latter are 3.72m apart and are coped with chamfered limestone blocks. They terminate in out-projecting piers (the SE one is missing). The carriageway is curved.					
Interest	None					
Evaluation	The architectural character of this bridge is diminished by the fact that most of it is buried. It nevertheless survives intact and also has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

03206_01 FWH 31/07/2007 West elevation of bridge.



Site **03207**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03207	01	Type	Level crossing		
County	Louth		OS 6-inch map	LH007	Grid	301635 305843
Surveyed?	N					

Site 03208

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03208 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH007	Grid	301246	305756
Surveyed?	N					

Site 03209

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03209 01	Type	Bridge (rail/road)			
County	Louth	OS 6-inch map	LH006	Grid	300848	305733
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	An arched bridge carries the embanked single-track line over an accommodation track. It is of split random rubble construction. The abutments are 9.81m deep. The arch is of semicircular profile and spans 3.03m. It is embellished with rusticated and margined voussoirs. The actual soffit is of brick. There is also a chamfered string course through the arch and around the quoins at spring level. The spandrels have dressed stone copings. There are wing walls at each end, the chamfered copings of which are largely missing. These walls terminate in low piers, now partly buried (and missing at the SW end).					
Interest	Architectural; Historical					
Evaluation	This bridge is of architectural interest and also has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

03209_01 FWH 31/07/2007 Bridge from SE.



03209_02 FWH 31/07/2007 North elevation.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03210 01	Type	Bridge (rail/road)			
County	Louth	OS 6-inch map	LH006	Grid	300562	305784
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	

Description This skew arch bridge formerly carried the embanked single-track line over a road. Save for its north abutment, it has been demolished to enable the dog-leg approaches to be straightened and widened. The surviving abutment is constructed with rusticated limestone blocks laid to regular courses and with margined quoins. A string course runs across the top of the abutment and around the quoins at arch spring level. Vestiges of the masonry voussoir show it to comprise rusticated blocks with margined edges. The actual soffit was of skewly-laid brick, the bottom course of which survives above the string course. A curved wing wall survives at the west end of the abutment. It is of squared random rubble and terminates in a rusticated and margined pier. The quoins of the wing wall at the abutment end are finely dressed quoins on one face and rusticated and margined on the other. There may be a wing wall at the other end, but dense overgrowth prevented close inspection.

Interest None

Evaluation No special industrial heritage significance.

Significance Record only

03210_01 FWH 31/07/2007 Remains of north abutment from SE.



Site 03211

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03211	01	Type	Level crossing		
County	Louth		OS 6-inch map	LH006	Grid	300147 305953
Surveyed?	Y		Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	The former line has been reclaimed for agriculture and no traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03212

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03212 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH006	Grid	299827	306036
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	This only remnant of this accommodation crossing is wrought-iron sunburst gate hung from a random rubble pillar on the south side of the former line.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03212_01 FWH 31/07/2007 Gate and surviving post on south side of line.



Site **03213**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03213	01	Type	Level crossing		
County	Louth		OS 6-inch map	LH006	Grid	299395 306083
Surveyed?	N					

Site **03214** **Carrickallan Crossing; Carrickallan Halt**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959. This crossing was a railcar stop from c.1935 to 1940s.

03214_01 B. Clarke Source Paddy Belton.
Level crossing house, 1950s.



03214_02 B. Clarke Source Paddy Belton.
Train at crossing, 1950s.



Component	03214 01	Type	Level crossing		
County	Louth	OS 6-inch map	LH006	Grid	299087 306116
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond
Completeness	Traces	Condition	Fair	Use	Disused
Description	Of this road crossing, only a single gate pillar survives on the south side of the former line, west of the road. It is of random rubble and square in cross section. The former line has been reclaimed for agricultural use hereabouts.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

Component	03214 02	Type	Level crossing house			
County	Louth	OS 6-inch map	LH006	Grid	299070	306108
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A detached two-bay, single storey level crossing keeper's house aligned E-W on the south side of the former line, west of the road. It has been extensively refurbished. Gabled natural slate roof with central rebuilt brick chimney. The walls are harled with cement. There is a square-headed window to each bay on the south elevation; they have uPVC windows and may be later insertions. Both gables are blank. A later flat-roofed extension has been added to the north elevation. The exposed section of wall has a window. Old photographs show this to have been the original entrance door which was relocated to the extension.					
Interest	None					
Evaluation	The alterations to this house have altered its original character to such an extent that it is now of no special industrial heritage significance.					
Significance	Record only					

Component	03214 03	Type	Hardstand			
County	Louth	OS 6-inch map	LH006	Grid	299097	306105
Surveyed?	Y	Survey date	31/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No hardstand associated with this request halt is evident.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

03214_01 FWH 31/07/2007 House from NE.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03215 01	Type	Bridge (road/rail)			
County	Louth	OS 6-inch map	LH006	Grid	298734	306153
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This bridge carried an accommodation track over the railway. It has been demolished and the cutting infilled. In the farmyard at the end of the track are two GNR plaques mounted in the wall (i.e. not in situ). One is an oval plaque carrying the number '11'. This was removed from Cracker's Bridge. The other reads "Great Northern Railway Co (l)/ Hereby give public warning/ to all persons not to/ trespass upon this railway/ Trespassers will be prosecuted".					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

03215_01 FWH 03/08/2007 Plaque from Cracker's Bridge.



03215_02 FWH

03/08/2007 'Trespassers Prosecuted' sign.



Site

03216

Kellybridge Halt (KBE)

History

Opened as a halt by GNR(I) in 1924. Closed by Great Northern Railway Board in October 1957.

03216_01 R M Arnold 06/04/1955 Source C. Friel Collection.

Kellybridge Halt with Clones to Dundalk train.



Component	03216 01	Type	Platform		
County	Louth	OS 6-inch map	LH006	Grid	298175 306185
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	A platform was associated with this halt, at the SE corner of Killycronney Bridge. No traces survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03217	01	Type	Bridge (road/rail)	
County	Louth		OS 6-inch map	LH006	Grid 298147 306191
Surveyed?	Y		Survey date	03/08/2007	Surveyor Fred Hamond
Completeness	Complete		Condition	Good	Use Road bridge
Description	<p>This slightly skew bridge carries a road over the single-track line. The abutments are of squared random rubble, with margined quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of semi-elliptical profile and spans 9.30m measured orthogonally, and 9.44m on the skew. The voussoirs are rusticated, margined and vee-jointed. The soffit is of brick, laid orthogonally. The spandrels are detailed as the abutments. A chamfered stone string course runs across the tops of the spandrels. The parapets are 7.69m apart. The west one is detailed as the spandrels and has a dressed stone coping. The east one has been replaced in mass concrete. There are wing walls to each end, of squared random rubble, with dressed stone copings and small terminal piers. The carriageway is slightly curved and the approach roads are ramped.</p>				
Interest	Architectural; Historical				
Evaluation	This well executed bridge has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.				
Significance	Regional			Action	RPS
Other dbases	NIAH 13900608				

03217_01 FWH 03/08/2007 East elevation.



Site 03218

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03218 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH006	Grid	297630	306050
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03219

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03219 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH006	Grid	296904	305990
Surveyed?	N					

Site **03220**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03220	01	Type	Level crossing		
County	Louth		OS 6-inch map	LH006	Grid	296561 305982
Surveyed?	N					

Site 03221

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03221	01	Type	Level crossing		
County	Louth		OS 6-inch map	LH006	Grid	296173 305898
Surveyed?	Y		Survey date	03/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	Nothing survives of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03222

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03222 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH006	Grid	295983	305847
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this accommodation crossing. An access road now runs along the line of the track.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03223 01	Type	Bridge (road/rail)		
County	Louth	OS 6-inch map	LH006	Grid	295552 305753
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond
Completeness	Complete	Condition	Good	Use	Road bridge
Description	This skew bridge carries a road over the single-track line, here in a cutting. The abutments are of squared random rubble, with dressed plain quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of semi-elliptical profile and spans 9.20m measured orthogonally, and 9.63m on the skew. The voussoirs are rusticated and margined. The soffit is of random rubble. The spandrels are detailed as the abutments. A stone string course runs across the tops of the spandrels. The parapets, which are 7.85m apart, are of random rubble and have chamfered oversailing copings. They splay out to out-projecting piers. There are curved random rubble wing walls to each end, with dressed stone copings. The carriageway is slightly curved. A concrete block wall has been built across the east end of the arch. The cutting has been infilled east of the bridge but survives at west.				
Interest	Architectural; Historical				
Evaluation	This well executed bridge has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.				
Significance	Regional			Action	RPS
Other dbases	NIAH 13900611				

03223_01 FWH 03/08/2007 West elevation.



Site 03224

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03224 01	Type	Level crossing			
County	Louth	OS 6-inch map	LH006	Grid	295281	305766
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03225

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03225 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	293815	306776
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03226 01	Type	Siding		
County	Monaghan	OS 6-inch map	MO029	Grid	293796 306774
Surveyed?	Y	Survey date	28/09/2006	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing survives of this siding. On the south side of the track at the mill end is a corrugated metal shed, open to the railway side and mounted on a mass concrete and rubble stone base. It was used for the temporary storage of grain and meal.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				
Other dbases	MOIAR-029-003				

Site 03227

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03227 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	293540	306952
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History

Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

According to Mills (1903), the station building (component 1) was substantially remodelled in the period 1877-1903 by W.H. Mills, the GNR(I)'s Chief Engineer at that time. This may well have taken place when the Carrickmacross line opened in 1886. The footbridge (2c), houses (9) and signal cabins (6 and 10) also date from this period.

Old photographs show the footbridge to have been of the lattice girder type and the signal box (6) to have been a standard timber cabin over a brick base. The water tank (7) was a metal tank on a masonry base. The turntable (8) was used to turn the locos on the Carrickmacross branch line.

Patrick Kavanagh (1904-67) was born near here (in Mucker Townland) and mentions the railway in passing in several of his poems - 'The Great Hunger' and 'In Memory of my Mother'.

03301_01 Unknown 19/12/1957 Source C. Friel Collection.

Inniskeen Station, looking east from the signal cabin.



03301_02 H.B. Priestley 11/04/1956 Source C. Friel Collection: HBP29.

Inniskeen Station, seen from a Dundalk to Clones train.



03301_03 SLS Collection Source Ferris 2003, p.94.

An 0-6-0 shunts goods at Inniskeen Station.

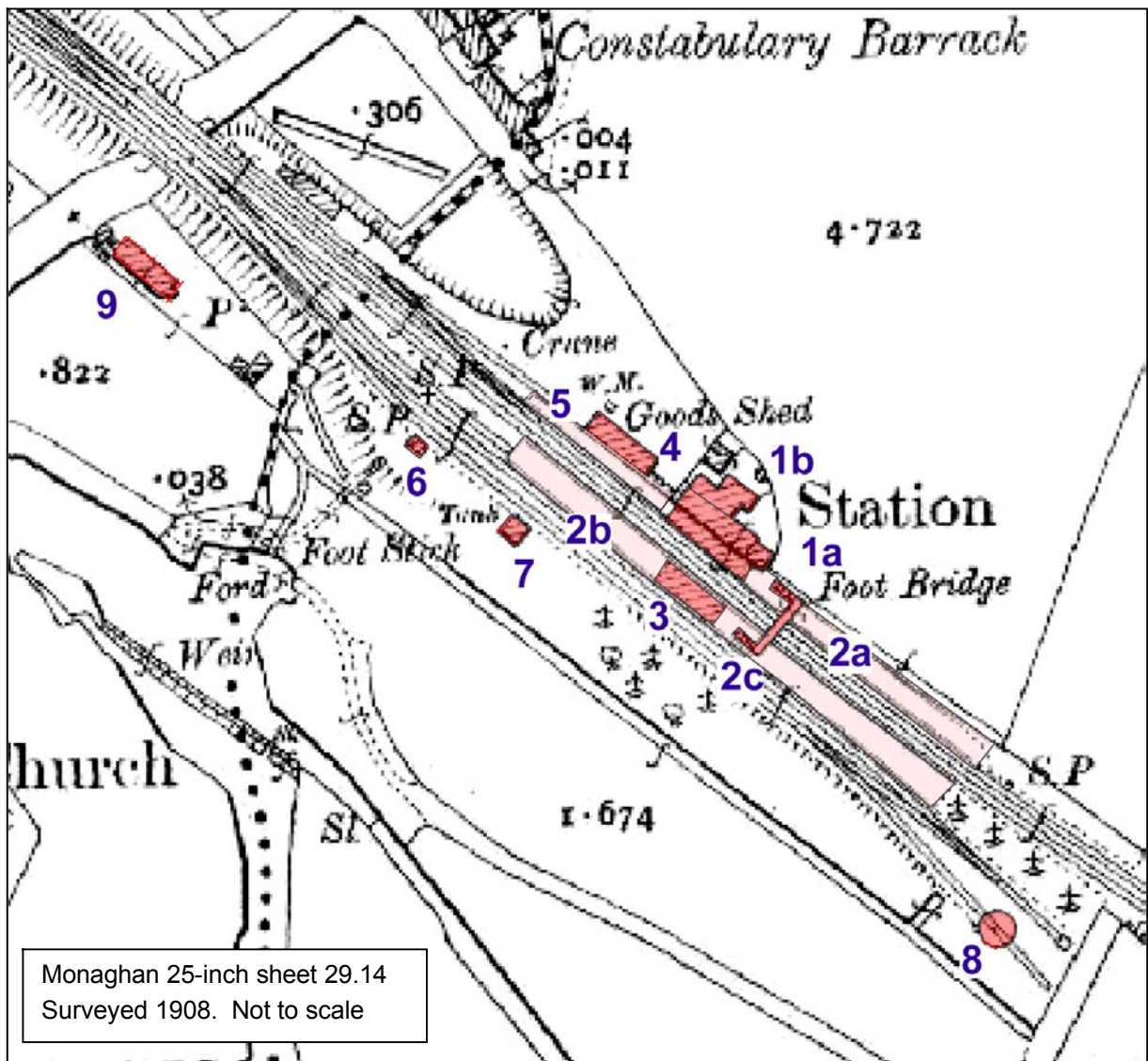
Inniskeen Station.



Inniskeen Station, looking west from the footbridge.



Component	03301 01a	Type	Station building			
County	Monaghan	OS 6-inch map	MO029	Grid	293450	307032
Surveyed?	Y	Survey date	25/09/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	<p>Former station building, with station house adjoining to rear and canopy to platform along SW elevation. Refurbished as a private dwelling in late 1990s, with new extension to NW gable.</p> <p>Hipped slate roof to station building with re-built redbrick chimneystacks. Roof height was raised and dormer windows inserted to accommodate new first floor level during refurbishment works. Replacement rainwater goods. Cement rendered random rubble walls with ruled and lined detail. Walls rendered during refurbishment works. Square-headed window openings with mixture of limestone and concrete sills, architrave surrounds and replacement two-over-two sash windows. Square-headed door opening to SW platform elevation with architrave surround and replacement timber panelled door. Modern sun room built onto NW end gable.</p> <p>The original canopy is in situ. It was originally supported on brackets mounted on the wall of the station building. However, cast-iron uprights have been added by the present owner during refurbishment; these were taken from the former canopy on the island platform. Because the platform slopes slightly downwards towards Dundalk, packing pieces have been had to be welded into each column. The maker's name is carried on the base of each column – E. Manisty, Engineers, Dundalk.</p>					
Interest	Historical; Group					



Evaluation This building has been heavily refurbished and enlarged to such an extent that little of its railway architecture survives apart from the chimney and canopy (modified with the addition of the iron columns). It is primarily of local industrial heritage merit on account of its railway associations and group value of the complex of which it is the most prominent part. Having said that, it and the station house is a Protected Structure (RPS 41402911). The poet Patrick Kavanagh lived near Inniskeen and cites the station in some of his work.

Significance Local Protection Monaghan
RPS
41402911

Component	03301	01b	Type	Station house		
County	Monaghan	OS 6-inch map	MO029	Grid	293450	307040
Surveyed?	Y	Survey date	25/09/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	<p>Former station house abuts NE elevation of station building and is contemporary with it. Recently refurbished and now internally linked with the former for use as a private dwelling.</p> <p>Pitched slate roof with replacement rainwater goods. Cement rendered walls (over brick) with ruled and lined detail. Square-headed window openings with architrave surrounds, mixture of stone and concrete sills and replacement uPVC windows. Modern extension to NW gable.</p>					
Interest	Historical; Group					
Evaluation	<p>This building has been heavily refurbished and little of its original character remains. It is primarily of local industrial heritage significance on account of its historical interest and group value with the overall complex. Having said that, it is, along with the station building, a Protected Structure (RPS 41402911).</p>					
Significance	Local	Protection	Monaghan RPS 41402911			

Component	03301 02a	Type	Platform			
County	Monaghan	OS 6-inch map	MO029	Grid	293474	307008
Surveyed?	Y	Survey date	25/09/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	This was the 'up' platform. It is of coursed rubble limestone. Inserted half-way up its face at the station building is an indented stone to provide a footing to facilitate access from the track bed to the platform. There is also a corresponding stone on the island platform. The concrete surface to the platform at station building may obscure sandstone kerbstones which are visible further along at SE. Modern outbuildings erected against platform wall at this end.					
Interest	Historical; Group					
Evaluation	This platform is of local industrial heritage significance on account of its historical interest and group value with the station complex.					
Significance	Local					

Component	03301 02b	Type	Platform			
County	Monaghan	OS 6-inch map	MO029	Grid	293466	306997
Surveyed?	Y	Survey date	25/09/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	This was the 'down' platform to Clones. It also served the Carrickmacross Branch and was formerly accessed by a metal footbridge. Coursed rubble limestone walls with granite kerbstones to edge. Rebuilt to NW end and kerbstones missing. A stone mid-way up platform wall opposite station building has hole carved into it to accommodate the toe of a shoe to allow easier access from tracks up onto platform. Grass to surface of platform, concrete to surface where canopy formerly stood.					
Interest	Historical; Group; Rarity					
Evaluation	This platform is of local industrial heritage significance on account of its historical interest and group value with the station complex. It is also the only surviving island platform in counties Monaghan or Louth.					
Significance	Local					

Component	03301 02c	Type	Bridge (foot/rail)			
County	Monaghan	OS 6-inch map	MO029	Grid	293463	307009
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this bridge survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03301 03	Type	Canopy shelter			
County	Monaghan	OS 6-inch map	MO029	Grid	293442	307017
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Fair	Use	Disused	
Description	The markings of this canopy are still visible on the island platform. Several dismantled cast-iron columns remain, not in situ; the others have been reused to support the canopy against the station building. The columns are hollow and doubled as rainwater spouts. The curved metal trusses which are beside these columns are modern and from elsewhere.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03301 04	Type	Goods shed			
County	Monaghan	OS 6-inch map	MO029	Grid	293430	307050
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this goods shed survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03301 05	Type	Goods beach			
County	Monaghan	OS 6-inch map	MO029	Grid	293420	307058
Surveyed?	Y	Survey date	25/09/2007	Surveyor	AR&R	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	The now-demolished goods shed formerly adjoined this goods beach to the SW of the station building. Rubble limestone wall with some sandstone kerbstone surviving and asphalt surface. There was formerly a weighing machine and crane.					
Interest	Historical; Group					
Evaluation	This beach is of local industrial heritage significance on account of its association with the railway complex.					
Significance	Local					

Component	03301 06	Type	Signal box			
County	Monaghan	OS 6-inch map	MO029	Grid	293404	307029
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this box survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03301 07	Type	Water tank			
County	Monaghan	OS 6-inch map	MO029	Grid	293422	307013
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this tank.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03301 08	Type	Turntable			
County	Monaghan	OS 6-inch map	MO029	Grid	293512	306947
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this turntable survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03301 09	Type	Railway-related house			
County	Monaghan	OS 6-inch map	MO029	Grid	293321	307089
Surveyed?	Y	Survey date	25/09/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	<p>A terrace of three two-bay single-storey former rail-related houses situated on main road, SW of station. Originally had entrance porches to gables of end houses and to NE elevation of middle house. The frontages (NE elevation) face the railway embankment. The end house at NW has been extended.</p> <p>Pitched slate and fibre cement roofs with three redbrick chimneystacks. The one on the SW gable is modern and the one on the SE house has been removed. Some cast-iron rainwater goods. Rendered and pebble-dashed walls. Square-headed window openings to end of terrace houses with replacement concrete sills and replacement uPVC windows. Central house retains segmental-headed window openings and painted granite sills.</p>					
Interest	Historical; Group					
Evaluation	These houses are relatively simple in design and are similar to a terrace at Rockcorry. They have been extensively refurbished and are now only of local industrial heritage significance because of their historical association with the railway and group value with the rest of the station complex.					
Significance	Local					

Component	03301 10	Type	Signal box		
County	Monaghan	OS 6-inch map	MO029	Grid	293592 306908
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing survives of this signal box beyond the Dundalk approach to the station. .				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03301_01 FWH 02/07/2007 Station building from west.



03301_02 AR&R 25/09/2007 Station building from south.



03301_03 AR&R 25/09/2007 West elevation of station building.



03301_04 FWH 02/07/2007 Station house from NW.



03301_05 AR&R 25/09/2007 East (rear) elevation of station building.



03301_06 AR&R 25/09/2007 Canopy to front elevation.



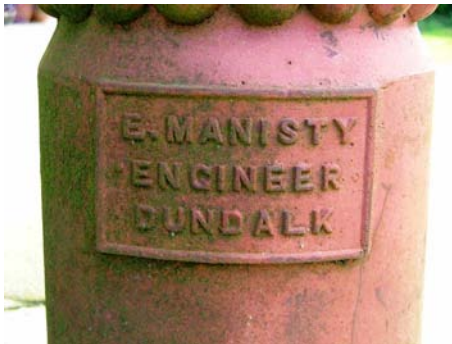
03301_07 AR&R 25/09/2007 Detail of cast-iron post to canopy.



03301_08 AR&R 25/09/2007 Detail of iron-work to canopy.



03301_09 AR&R 25/09/2007 Foundry name on cast-iron post.



03301_10 AR&R 25/09/2007 Door to station building.



03301_11 AR&R 25/09/2007 Window cill on front elevation of station building.



03301_12 AR&R 25/09/2007 Window to front elevation of station building.



03301_13 AR&R 25/09/2007 Station house from NW.



03301_14 AR&R 25/09/2007 East gable of station house.



03301_15 AR&R 25/09/2007 Chimneystack to station house.



03301_16 FWH 02/07/2007 Canopy shelter columns in front of platform.



03301_17 AR&R 25/09/2007 Up platform at station building.



03301_18 AR&R 25/09/2007 Up platform area in farmyard to SE of station building.



03301_19 AR&R 25/09/2007 Detail of platform wall at station building.



03301_20 AR&R 25/09/2007 Kerbstone to platform in farmyard.



03301_21 AR&R 25/09/2007 Sandstone block in platform wall allowing easy access from tracks.



03301_22 AR&R 25/09/2007 Platform with station building in background.



03301_23 AR&R 25/09/2007 Island platform with missing kerbstones to central section.



03301_24 AR&R 25/09/2007 Surface of island platform from southeast end.



03301_25 AR&R 25/09/2007 NW end of island platform.



03301_26 AR&R 25/09/2007 Detail of island platform wall with sandstone kerbstones.



03301_27 AR&R 25/09/2007 Sandstone kerbstones to island platform.



03301_28 AR&R 25/09/2007 Sandstone block in island platform wall allowing easy access from tracks



03301_29 AR&R 25/09/2007 Detail of sandstone block on island platform wall.



03301_30 AR&R 25/09/2007 General view of goods beach.



03301_31 AR&R 25/09/2007 View of area of goods beach with missing kerbstones.



03301_32 AR&R 25/09/2007 Detail of goods peach wall.



03301_33 AR&R 25/09/2007 Sandstone kerbstones on goods beach.



03301_34 AR&R 25/09/2007 Sandstone steps adjacent to goods beach.



03301_35 AR&R 25/09/2007 General view of rear (SE) elevation of houses.



03301_36 AR&R 25/09/2007 Detail of central house.



03301_37 AR&R 25/09/2007 Detail of window opening.



03301_38 AR&R 25/09/2007 Redbrick chimneystack.



Site 03302

History Line opened by the Dundalk & Enniskillen Railway in February 1849. The bridge was widened to accommodate the Carrickmacross branch, opened in 1886. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03302 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO029	Grid	293330	307119
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	
Description	<p>This arched bridge carries the single-track line over a road just west of Inniskeen Station. A metal beam extension was subsequently added to carry the Carrickmacross Branch (4102).</p> <p>The original section of the bridge, at NE, is of split random rubble throughout. Its abutments have plain dressed quoins and are 9.23m deep. A chamfered stone string course runs under the arch at spring level and around the exposed quoins. The arch is of segmental profile and spans 7.67m. It has dressed, vee-jointed voussoirs and a random rubble soffit. The NE parapet survives and has dressed oversailing copings and ends; the other parapet has been removed. The angled wing walls at NE have identical copings.</p> <p>The bridge has been extended by 3.91m at its SW end to carry the single-track Carrickmacross Branch line. The abutments to this section are of random rubble with dressed margined quoins; they are also 7.67m apart. Of the actual span, the cut-off ends of a 107cm x 30cm (42in x 12in) riveted metal beam survive at both ends. There is also ashlar terminal piers with a chamfered string course. The absence of a second beam suggests that the other edge of the track was also carried on the original bridge. There are angled wing walls at this end, of random rubble and with oversailing stone copings. The SE wall has been partly removed to facilitate access to the rear of adjacent houses.</p>					
Interest	Architectural; Historical; Technical; Setting					
Evaluation	<p>This bridge is primarily of interest in that it reflects both the original line and its extension to Carrickmacross. This is particularly evident in the arch and beam spans. It is also a prominent feature on the northern approach to Inniskeen. It is regarded as being of local interest in the Co Monaghan Development Plan (Local 56). However, it is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures. It is regarded as being of local interest in the Co Monaghan Development Plan (Local 56).</p>					
Significance	Regional	Protection	Monaghan RPS Local 56			

03302_01 FWH 03/08/2007 NE elevation.



03302_02 FWH 03/08/2007 SW elevation.



03302_03 FWH 02/07/2007 SW abutment of railway bridge.



03302_04 FWH 03/08/2007 Metal beam detail at SE end.



Site 03303

History The junction (strictly a divergence) opened in 1886 to serve the Carrickmacross branch. The signal box possibly dates to this time as well.

03303_01 E.M. Patterson 31/07/1958 Source Patterson 2003, p.90.
 Junction of Enniskillen line (at right) with Carrickmacross branch (left).



Component	03303 01	Type	Junction			
County	Monaghan	OS 6-inch map	MO029	Grid	293283	307158
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	The earthworks associated with where the two lines diverged have been removed.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03303 02	Type	Signal box			
County	Monaghan	OS 6-inch map	MO029	Grid	293275	307175
Surveyed?	Y	Survey date	03/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of signal box survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History

Line opened by the Dundalk & Enniskillen Railway in February 1849. The crossing was used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

The level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

03304_01 Unknown

Source Mary McDermott.

Level crossing house prior to refurbishment.



Component	03304 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	292325 307950	
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this road crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03304 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO029	Grid	292342	307945
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A two-bay, one-and-a-half storey former level crossing keeper's house aligned NW/SE at the SW corner of the line. Known as "The Old Railway gatehouse". Extensively refurbished and extended by its current owner c.1997. Replacement gabled roof, cement-rendered walls (over random rubble), and square-headed openings with replacement door and window frames. Porch (an original feature) on NW gable.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03304 03	Type	Hardstand			
County	Monaghan	OS 6-inch map	MO029	Grid	292335	307945
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No hardstand associated with this request halt is evident.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

03304_01 FWH 09/08/2007 House from NE.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03305 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO029	Grid	292142	308103
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	
Description	An arched bridge carries the embanked line over the Fane River. The abutments are of squared random rubble, embellished with dressed and margined quoins. They are estimated to be c.8.5m deep. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of segmental profile and spans 10.77m. The voussoirs are of rusticated and margined vee-jointed blocks and the soffit of random rubble. The spandrels are detailed as the abutments. The SW parapet has been replaced with a mass concrete one, but the original masonry string course runs along its base. The NE parapet and string course are both missing. There are squared random rubble wing walls to each end; their chamfered stone copings are missing except on the upstream left-bank end. The bridge now carries the Monaghan Way long-distance footpath.					
Interest	Architectural; Historical; Setting					
Evaluation	This bridge is of architectural interest because of its size and quality of construction. It is second only to the bridge over the River Finn in size (3816) and a feature of the Monaghan Way. It also has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03305_01 FWH 09/08/2007 Downstream (SW) elevation.



03305_02

FWH

09/08/2007

Left-bank (SE) abutment and soffit detail.



Site 03306

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03306 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	291839	308409
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive. In the yard of the farmyard a short distance to the west are two wrought-iron sunburst gates, probably from this crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03307

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03307 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	291727	308583
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	A square random rubble pillar survives at the SW end of this accommodation crossing survive. On the opposite side of the former line is a wrought-iron sunburst gate hung from a timber post.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03307_01 FWH 09/08/2007 Surviving gate from SE.



Site 03308

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03308 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO029	Grid	291565	308879
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation bridge and its adjoining embankments survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03309

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03309 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	291207	309258
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site **03310** **Blackstaff Crossing; Blackstaff Halt (BSF)**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Used as a railcar halt from August 1927. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.
The level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

Component	03310 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	290738	309605
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03310 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO029	Grid	290722	309608
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A two-bay, one-and-a-half storey former level crossing keeper's house aligned NW/SE at the SW corner of the line. Extensively refurbished and extended to NW and at rear. Replacement gabled cement-tile roof, rebuilt chimney (on original party wall), cement-rendered walls, and square-headed openings with replacement door and window frames (with concrete cills). Porch at road end of track (NE) elevation.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03310 03	Type	Platform		
County	Monaghan	OS 6-inch map	MO029	Grid	290752 309587
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond
Completeness	Complete	Condition	Poor	Use	Disused
Description	On SE side of the crossing, along the SW side of the line. Remains. The Monaghan Way runs along the track bed hereabouts. RESURVEY - check Murray photo.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03310_01 FWH 09/08/2007 House from SE.



03310_02 D Murray 23/08/2006 Platform, from west.



Site 03311

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03311 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO029	Grid	290449	309933
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched bridge carries the embanked single-track railway over an accommodation track. The abutments, which are 9.14m deep, are of squared random rubble, embellished with rusticated and margined quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of semicircular profile and spans 3.64m. Its soffit is of brick, but the voussoirs are of rusticated stone blocks with a margined edge to the arch. The voussoirs are stepped into the spandrels which are detailed as the abutments. They are coped with a chamfered stone string course. There are squared random rubble angled wing walls at each end, coped with rounded dressed stone copings. At the SW end of the bridge, is a wrought-iron sunburst gate leading into a farmyard. It is hung between two square random rubble pillars.

Interest Architectural; Historical

Evaluation This is a good example of its type and also has a historical association with the railway. It is of local industrial heritage significance.

Significance Local

03311_01 FWH 09/08/2007 SW elevation of bridge.



03311_02 FWH

09/08/2007 Adjoining farmyard gate at NW end of bridge.



Site 03312

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03312 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO029	Grid	290135	310353
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	On the NE side of the line is a wrought-iron sunburst gate hung between two rendered random rubble and brick piers. There is a similar gate on the SW side of the line, but only one pier.					
Interest	None					
Evaluation	The relative completeness of these gates and their historical association with the railway make them of local industrial heritage significance.					
Significance	Local					

03312_01 FWH 09/08/2007 Gate on NE side of track.



Site 03313

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03313 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO028	Grid	289932	310632
Surveyed?	N					

Site 03314

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03314 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO028	Grid	289819	310784
Surveyed?	N					

Site 03315

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03315 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO028	Grid	289504	311202
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	Only the NW abutment of this accommodation bridge survives. Its face has fallen away to reveal a random rubble core. A galvanised steel staircase runs up the right-hand side of the abutment to the top of the embankment, along which the Monaghan Way runs.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03315_01 FWH 09/08/2007 Remains of abutment.



Site 03316

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03316 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO028	Grid	289338	311423
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	
Description	A wrought-iron sunburst gate survives on both sides of the track, hung from a rendered square pillar. The corresponding pillar at the other end of each gate is missing.					
Interest	Architectural; Historical					
Evaluation	The relative completeness of these gates and their historical association with the railway make them of local industrial heritage significance.					
Significance	Local					

03316_01 FWH 09/08/2007 Gate at SW side of line.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03317 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO028	Grid	288429	312629
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Foot bridge	
Description	An arched bridge carries the embanked single-track railway over a road. The abutments, which are 9.14m deep, are of rusticated masonry blocks, laid to regular courses and embellished with margined quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of semicircular profile and spans 5.88m. The soffit is of brick and the voussoirs of dressed and vee-jointed stone blocks stepped into the spandrels. The latter are of squared random rubble, as are the parapets. A chamfered stone string course runs along the base of the parapets. The parapets are coped with oversailing dressed stone copings. There are angled squared random rubble wing walls at each end, similarly coped and terminating in small piers. The east end of the SW parapet is missing. The Monaghan Way long-distance footpath follows the track bed over the bridge.					
Interest	Architectural; Historical; Setting					
Evaluation	This well executed bridge occupies a prominent setting and has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03317_01 FWH 09/08/2007 NE elevation.



03317_02 FWH

09/08/2007 SW elevation.



History

Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

The signal cabin (3), which dates from the period 1877-1903 (and was the work of W.H. Mills), was superseded sometime after 1908 by another on the 'down' platform (4). The waiting shelter (5) and station houses (9) also date from the same period. At the same time, the line was doubled at the station; this would have necessitated the building of a platform on the down side, opposite the station building.

03401_01 R.M. Arnold 09/01/1954 Source C. Friel Collection.

Culloville Station, looking west.



03401_02 Drew Donaldson Source C. Friel Collection.

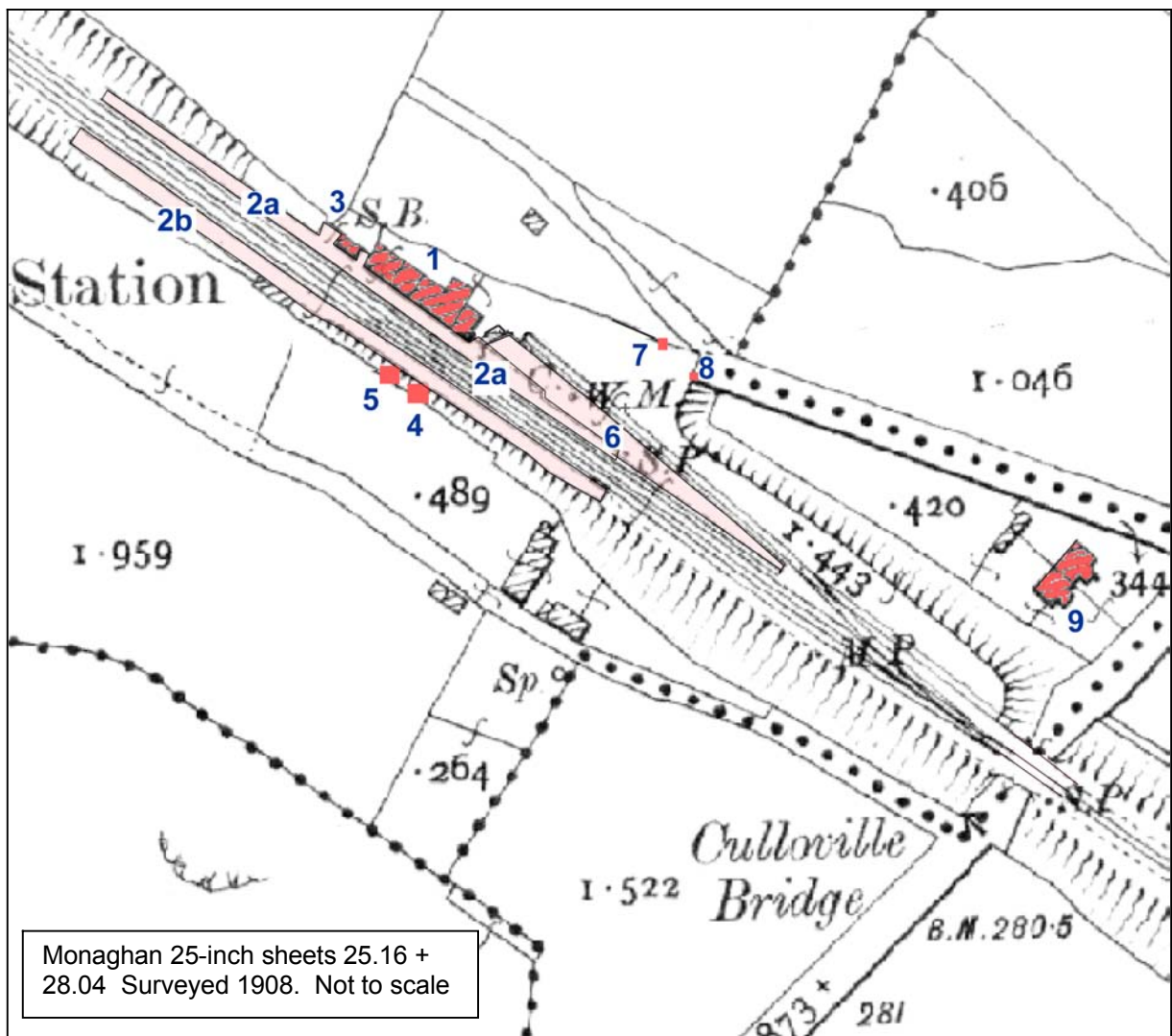
Dundalk-bound Bundoran Express at Culloville.



03401_03 SLS Collection Source Ferris 2003, p.95.

Culloville Station from east end.

Component	03401 01	Type	Station building			
County	Monaghan	OS 6-inch map	MO025	Grid	288304 312718	
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	A rendered wall is all that remains of the station building which was demolished some years ago. The render appears to be original, but there are no openings,					



suggesting that it was the back wall, built into the hillside which has since been regraded, probably when the track bed was surfaced for a road.

Interest None
 Evaluation No industrial heritage significance.
 Significance Record only

Component	03401 02a	Type	Platform
County	Monaghan	OS 6-inch map	MO025 Grid 288281 312731
Surveyed?	Y	Survey date	02/07/2007 Surveyor Fred Hamond
Completeness	No visible remain	Condition	N/A Use N/A
Description	This was the 'up' platform, of which nothing remains, having been cleared to facilitate a new accommodation road along the track bed.		
Interest	None		
Evaluation	No industrial heritage significance.		
Significance	Record only		

Component	03401 02b	Type	Platform
County	Monaghan	OS 6-inch map	MO025 Grid 288276 312722
Surveyed?	Y	Survey date	29/10/2007 Surveyor AR&R
Completeness	Complete	Condition	Poor Use Disused
Description	This was the 'down' platform. It comprises a coursed rubble stone platform wall with a concrete kerbstone crudely reinforced with metal bars. Concave moulding to edge of kerb.		
Interest	Historical; Group		
Evaluation	Has group value in the context of the other surviving remains and is the most tangible evidence of the presence of the former station. It is of local industrial heritage significance.		
Significance	Local		

Component	03401 03	Type	Signal box			
County	Monaghan	OS 6-inch map	MO025	Grid	288294	312725
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives at this location.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03401 04	Type	Signal box			
County	Monaghan	OS 6-inch map	MO028	Grid	288304	312707
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	Redbrick base of signal box survives, laid in English bond, on down platform. Segmental-headed openings to elevation facing platform with concrete sills. No glazing or glazing bars survive. Square-headed door opening with brick flat-arch. Exterior concrete steps access door. Remains of timber ceiling and floor from former signal box cabin have collapsed into lower floor area. The points rods ran under the platform. The jambs on the rail face of this opening are of brick and the lintel of reinforced concrete – clearly inserted into an existing rubble masonry wall. There are also flat- and bull-headed rails supporting the lintel.					
Interest	Historical; Group					
Evaluation	What remains has group value in the context of the platform remains and is of local industrial heritage significance.					
Significance	Local					

Component	03401 05	Type	Waiting shelter			
County	Monaghan	OS 6-inch map	MO028	Grid	288297	312711
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	Remains of former waiting room located on the down platform at Culloville Station, adjacent to the signal box. Concrete plinth wall survives denoting the size and position of former structure, with sockets for the uprights to former timber superstructure.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03401 06	Type	Goods beach			
County	Monaghan	OS 6-inch map	MO028	Grid	288333	312705
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this beach which had a weighbridge and crane.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Local					

Component	03401 07	Type	Telegraph pole			
County	Monaghan	OS 6-inch map	MO025	Grid	288349	312717
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	Five telegraph poles survive in the vicinity of the former railway station, each a wooden pole with five timber cross bars with porcelain insulators.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03401 08	Type	Entrance (station)			
County	Monaghan	OS 6-inch map	MO028	Grid	288359	312710
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	Timber gate post with one timber gate supported by iron elements including iron hinges. Corresponding post on other side of road is missing.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03401 09	Type	Station house			
County	Monaghan	OS 6-inch map	MO028	Grid	288437	312669
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	<p>Pair of three-bay two-storey former railway house, with projecting gabled entrance porches to front elevation. Probably housed not only the station master, but also station employees. Now in use as one house. Located east of the station, set back on west side of main road.</p> <p>Pitched slate roof with terracotta ridge tiles and three redbrick chimneystacks. Replacement rainwater goods and eaves. Rendered walls with ruled and lined detail. Segmental-headed window openings with replacement timber casement windows. Painted continuous sill course to windows may incorporated sandstone sills. Window on front elevation has window sill level dropped to form new entrance. Pair of round-headed window openings on projecting gabled entrance porches. Square and segmental-headed window openings on rear elevation. One sandstone sill visible on rear elevation. Other sills are painted. Former door openings now blocked up were located on side elevations of projecting gabled entrance porches. Segmental-headed door opening on rear elevation.</p>					
Interest	Historical; Group					
Evaluation	Although this building has been extensively refurbished, its railway character is just about apparent. Its group value with the adjoining station raises it to local industrial heritage significance.					
Significance	Local					

03401_01 FWH 02/07/2007 Remains of station building, from SE.



03401_02 FWH 02/07/2007 Down platform from SE.



03401_03 AR&R 29/10/2007 Platform from NW.



03401_04 AR&R 29/10/2007 Surface of platform.



03401_05 AR&R 29/10/2007 Concrete kerbstones.



03401_06 AR&R 29/10/2007 Detail of damaged concrete kerbstone.



03401_07 AR&R 29/10/2007 Detail of platform wall.



03401_08 AR&R 29/10/2007 Concave edge to kerbstone.



03401_09 AR&R 29/10/2007 Terracotta drainage pipe in platform wall.



03401_10 FWH 02/07/2007 Signal box and points' roding opening under platform, from east.



03401_11 AR&R 29/10/2007 Signal box: redbrick jambs to platform



03401_12 AR&R 29/10/2007 Signal box: brick base



03401_13 AR&R 29/10/2007 Signal box: window opening



03401_14 AR&R 29/10/2007 Signal box: interior



03401_15 AR&R 29/10/2007 Signal box: interior towards windows



03401_16 AR&R 29/10/2007 Signal box: interior towards door



03401_17 AR&R 29/10/2007 Former waiting room on platform.



03401_18 AR&R 29/10/2007 Base of waiting room.



03401_19 AR&R 29/10/2007 Detail of concrete base to waiting room.



03401_20 FWH 02/07/2007 Telegraph pole near entrance.



03401_21 AR&R 29/10/2007 Entrance gate.



03401_22 AR&R 29/10/2007 Entrance gate post.



03401_23 AR&R 29/10/2007 Entrance gate hinge.



03401_24 AR&R 29/10/2007 Entrance gate latch.



03401_25 FWH 02/07/2007 Station house from SE.



03401_26 AR&R 29/10/2007 Front and side elevation of station house.



03401_27 AR&R 29/10/2007 Rear of station house.



03401_28 AR&R 29/10/2007 Central bay of station house.



03401_29 AR&R 29/10/2007 Projecting bay to front elevation of station house.



03401_30 AR&R 29/10/2007 Sandstone sill to window of station house.



Site 03402

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03402 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO025	Grid	288146	312828
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	
Description	<p>A triple-span beam bridge carries an accommodation track over the single-track line, here in a deep cutting just north of Culloville Station. The abutments and slender tapered piers are of rock-faced masonry blocks laid to regular courses and embellished with rusticated and margined quoins; they are 3.42m deep. The middle span measures 9.15m (at the base of the piers) and comprises two lattice metal box girders, the ends of which rest on dressed stone pillow blocks. These beams are c.76cm deep by 46cm wide (30in by 18in) and are set c.2.1m apart. Both the end sections comprise three riveted metal girders and span 5.50m (at the top of the piers). The two outer ones have solid webs of 'fish belly' profile and measure 61cm x 23cm at centre, and 38cm x 23cm at their ends (24in x 9in/ 15in x 9in). The middle beam is a simple lattice girder of 61cm x 23cm cross-section. These three beams are set 1.40m apart and also rest on dressed stone pillow blocks. The original deck was presumably timber but has been replaced with pre-cast hollow concrete slabs; it is 3.45m wide and projects beyond the supporting beams. The original five-bar tubular metal railings survives, carried on angled metal uprights. The railings terminate in ashlar piers topped with rounded copings.</p>					
Interest	Architectural; Historical; Technical; Setting; Rarity					
Evaluation	<p>This elegant bridge is of architectural and technical interest because of its scale, quality of construction and survival of its lattice girder spans. It has an historical association with the railway and occupies a prominent position along the cutting just north of the former station. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures. The spans also merit measured drawings.</p>					
Significance	Regional			Action	RPS; Measured drawings	

03402_01 FWH 09/08/2007 NW elevation.



03402_02 FWH 09/08/2007 SE elevation.



03402_03 FWH 09/08/2007 NW elevation, from west.



03402_04 FWH 09/08/2007 Looking NE along underside of middle span.



03402_05 FWH 09/08/2007 Looking NE along underside of end span at SW.



03402_06 FWH 09/08/2007 Looking NE across deck.



Site 03403

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03403 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	287860	313083
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03404

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03404 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	287505	313537
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site **03405**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03405 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	287319	313893
Surveyed?	N					

Site**03406****Ballymacarry Bridge**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03406 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO025	Grid	287210	314196
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of this skew railway bridge and its associated embankments have been removed. The road has also been widened.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03407

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03407 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO025	Grid	287091	314709
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road bridge survive. The cutting which it spanned has been infilled and the dog-leg on its approaches removed.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03408

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03408 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	286966	315007
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate on a square random rubble pillar on the west side of the former line is all that survives of this accommodation crossing.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03408_01 FWH 09/08/2007 Level crossing gate, from south. The line is at right, running north.



Site 03409

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03409 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO025	Grid	286840	315214
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	All that survives of this rail bridge over an accommodation track are the truncated remains of a random rubble abutment on the south side of the road. Two dressed granite blocks are set into the top of wall (pillow blocks for beam spans?) The embankment has been removed on the north side of the road.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03409_01 FWH 09/08/2007 Remains of bridge abutment.



Site**03410****Drumgoose Crossing; Drumgoose Halt**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.
The level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

Component	03410 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	286633	315443
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survive, the road having been widened.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03410 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO025	Grid	286616	315445
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A two-bay, single-storey former level crossing keeper's house aligned NW/SE at the SW corner of the line. Extensively refurbished and extended to NW. Replacement gabled artificial slate roof, cement-rendered walls, and square-headed enlarged openings with replacement window frames (with concrete cills). New entrance porch at corner with extension.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03410 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO025	Grid	286625 315442
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No hardstand associated with this request halt is evident.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03410_01 FWH 09/08/2007 Refurbished house, from east.



Site 03411

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03411 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	286410	315627
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03412

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03412 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO025	Grid	286231	315774
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of this bridge and its associated embankments have been removed to facilitate access along the accommodation track. According to the landowner, the span comprised four heavy-duty timber beams across the abutments.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03413

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03413	01	Type	Bridge (rail/road)		
County	Monaghan		OS 6-inch map	MO025	Grid	286077 315898
Surveyed?	Y		Survey date	09/08/2007	Surveyor	Fred Hamond
Completeness	Complete		Condition	Fair	Use	Disused

Description An arched bridge carries the embanked single-track line over a road. The abutments, which are 9.12m deep, are of square random rubble embellished with rusticated and margined quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of semicircular profile and spans 3.65m (this is the narrowest arched span over a public road on this line). The soffit is of brick and the voussoirs of dressed stone blocks with a margined edge to the arch. The voussoirs are stepped into the spandrels. The latter are of squared random rubble with chamfered stone coping. There are squared random rubble wing walls at each end, with curved stone copings; they terminate in small piers. The embankment has been removed at both ends to reveal two stone abutments.

Interest None

Evaluation This bridge is a good example of its type, albeit more akin to railway bridges over accommodation tracks. It also has a historical association with the railway and is of local industrial heritage significance.

Significance Local

03413_01 FWH 09/08/2007 NE elevation.



03413_02 FWH

09/08/2007 Bridge from east.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03414 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO025	Grid	285906	316025
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description A highly skewed arch bridge carries what was formerly the main road over the single-track line, here in a cutting. The abutments are of rock-faced masonry blocks laid to regular courses and embellished with rusticated and margined quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. There are shallow, similarly detailed buttresses to each side. The arch is of segmental profile and spans 9.15m measured orthogonally (and c.12-13m on the skew). The soffit is of skewly-laid brick and the voussoirs of dressed vee-jointed blocks stepped into the spandrels. The latter are of rock-faced, randomly-sized masonry blocks. The parapets, which are 9.41m apart, are of squared random rubble and coped with chamfered stone blocks. A chamfered stone string course runs along the base of the parapets. The approach roads are dog-legged. The cutting is infilled to the west and the south end of the west parapet has been removed to facilitate a new main road which bypasses the bridge. The original road is now used for house access only.

Interest Architectural; Historical

Evaluation This is a good example of a highly skewed brick and stone bridge which also has an historical association with the railway. Unfortunately it has been degraded by the removal of the parapet and infilling to the extent that it is now only of local industrial heritage significance.

Significance Local

03414_01 FWH 09/08/2007 East elevation.



03414_02 FWH

09/08/2007 Stonework detailing at SE end.



Site 03415

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03415 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	285662	316178
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03416

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03416 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO025	Grid	285544	316286
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched bridge carries the embanked single-track railway over an accommodation road. The abutments, which are 9.19m deep, are of squared random rubble, with rusticated and margined quoins. A chamfered stone string course runs through the arch at spring level and around the quoins. The arch is of semicircular profile and spans 3.65m. The soffit is of brick and the voussoirs of rusticated stone blocks with a margined edge to the arch. They are stepped into the spandrels. The latter are of squared random rubble with chamfered stone copings. There are squared random rubble wing walls at each end, similarly coped and terminating in small piers. The embankment has been removed at the SE end of the bridge.

Interest Architectural; Historical

Evaluation This is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03416_01 FWH 09/08/2007 SW elevation.



Site 03417

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03417 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	285335	316479
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive. The main road runs along the line of the track hereabouts.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site **03418** **Annadrumman Crossing; Annadrumman Halt**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03418 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	285115	316684
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive. A timber gatepost lies in the waste ground behind the level crossing keeper's house.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03418 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO025	Grid	285104	316685
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	A two-bay, single-storey former level crossing keeper's house aligned NW/SE at the NW side of the former road crossing. Gabled natural slate roof with yellow-brick chimney on party wall. Cement-harled brick walls, with structural cracks. Square-headed openings. The SE bay is three openings wide to the line (NE) elevation, with a doorway to centre and window opening to each side. There is also a window opening to this elevation of the NW bay and one to each gable. There are two window openings to the rear elevation of the SE bay. The surviving windows are all 2/2 sliding sashes and all these openings have concrete cills. Internally, there is a fireplace to both bays on their party wall. The SE bay was originally divided into two rooms by means of a stud wall, now removed.					
Interest	Architectural; Historical					
Evaluation	Although virtually intact, this house is in such poor condition that it is now only of local industrial heritage significance.					
Significance	Local					

Component	03418 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO025	Grid	285109 316684
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No hardstand associated with this request halt is evident.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03418_01 FWH 09/08/2007 House from east.



03418_02 FWH 09/08/2007 House from south.



03418_03 FWH

09/08/2007 Fireplace in NW bay.



Site 03419

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03419 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO025	Grid	284783	316996
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	A slightly skew beam span formerly carried the single-track line over an accommodation road. The deck has been lifted and only the abutments remain. They are of split random rubble, with rusticated and margined quoins. The overall abutment depth is 9.12m, but there is a raised section towards the north end, 2.95m wide and coped with dressed stones. The flanking sections are coped with concrete, which may have acted as pillow blocks for the beam spans. The span measures 2.79m orthogonally, and 2.95m on the skew.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03419_01 FWH 09/08/2007 NE elevation.



Site**03420****Carragartha Crossing; Carragartha Halt**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.
Level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

Component	03420 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	284506	317257
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03420 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO025	Grid	284494	317260
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives of the level crossing keeper's house which was on the NE side of the crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03420 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO025	Grid	284502 317257
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No hardstand associated with this request halt is evident.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Site **03421**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03421 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	284228	317518
Surveyed?	N					

Site **03422**

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03422 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO025	Grid	284084	317652
Surveyed?	N					

Site 03423

History Line opened by the Dundalk & Enniskillen Railway in February 1849. The original deck was probably renewed with plate girder spans on cylindrical piers in the 1920s. The line was closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03423 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO025	Grid	283673	318033
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	The embanked single-line track was formerly carried over an unnamed river on a metal beam bridge. Only the random rubble abutments survive, near the tops of the high embankment. That on the right (east) bank was inaccessible and heavily overgrown, so could not be surveyed in detail. The left (west) bank abutment has a brick wall of unknown function incorporated in its southern end. Towards the north end, a circular cast-iron column protrudes above the ground. It is capped with a bolted-down top and measures 1.6m in diameter. There were probably upwards of three beam spans between the abutments. The banks of the actual river are faced with random rubble.					
Interest	None					
Evaluation	No little of this once substantial bridge now survives to be of special industrial heritage significance.					
Significance	Record only					

03423_01 FWH 09/08/2007 Cast-iron column on left (west) bank.



Site 03424

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03424 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO025	Grid	283179	318779
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of this rail bridge and associated embankments have been cleared and the road widened.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03425

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03425 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO020	Grid	282848	319328
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of this skew road bridge have been removed and the cutting which it spanned infilled.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History Line opened by the Dundalk & Enniskillen Railway in February 1849. Cited as tunnel on 2nd edition OS map. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03426 01	Type	Tunnel		
County	Monaghan	OS 6-inch map	MO020	Grid	282655 319505
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond
Completeness	Complete	Condition	Fair	Use	Disused

Description This c.50m long rock-cut tunnel carried the line under the front garden of Connabury House. The abutments are of rock-faced stone blocks set in regular courses and have margined quoins. A chamfered stone string course runs the length of the tunnel at arch spring level and around the quoins. The tunnel is lined with a semicircular brick arch spanning 8.51m. The SE end has been blocked with a concrete block wall and modern sheds built against its external face. The NW end has rusticated voussoirs around the margined arch edge; these splay radially into the spandrels which are also rusticated. There are shallow rusticated buttresses to each side beyond which are side walls detailed as the abutments. A chamfered stone string course runs over the crown of the arch. The parapets (which are visible to both ends) are of randomly-sized dressed stone blocks with chamfered copings and out-projecting terminal piers. The SE parapet steps up the slope from the road end. There is an accommodation road to Connabury House at this end. The bed of the tunnel is now partly infilled and houses have been built along its line, just beyond its NW end.

Interest Architectural; Historical; Rarity

Evaluation This is the only tunnel on the entire line between Dundalk and Clones. Its well executed exposed faces are detailed as the bridges elsewhere on this line. It also has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Monuments and Places.

Significance Regional Action RMP

03426_01 FWH 09/08/2007 Looking SE inside tunnel.



03426_02 FWH 09/08/2007 NW end of tunnel.



03426_03 FWH 09/08/2007 NW elevation.



History Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03427 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	282425	319703
Surveyed?	Y	Survey date	09/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this road crossing just south of Castleblaney Station survives. Old photos show two pairs of timber gates between timber posts. Each gate has four horizontal bars, diagonal braces and metal rod diagonal hangers. The gates were controlled from the east signal box. New houses across line at road crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History

Line opened by the Dundalk & Enniskillen Railway in February 1849. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

In the period 1877-1903, the line was doubled at the station and necessitated the building of a platform on the down side (component 2b), opposite the station building. The footbridge (2c), waiting shelter (3) and signal boxes (6 and 8) date from the same period. Old photographs show the footbridge to have been a two-span lattice girder bridge (one span over line, the other a shorter span over platform on west side). At each end were two sets of steps – one to/from street, the other to/from platform. The signal boxes were of the standard GNR design - a timber cabin over a brick base. Old photographs show the water tank (7) to have been a metal tank on a brick or stone base.

03501_01 E.M. Patterson 31/07/1958 Source C. Friel Collection: EMP43P.

Castleblaney Station.



03501_02 Locomotive Publishing Co Source Patterson 2003, p.54.

Castleblaney Station looking west.

03501_03 Tony Price 19/12/1959 Source C. Friel Collection.

Castleblaney Station, looking towards Dundalk.



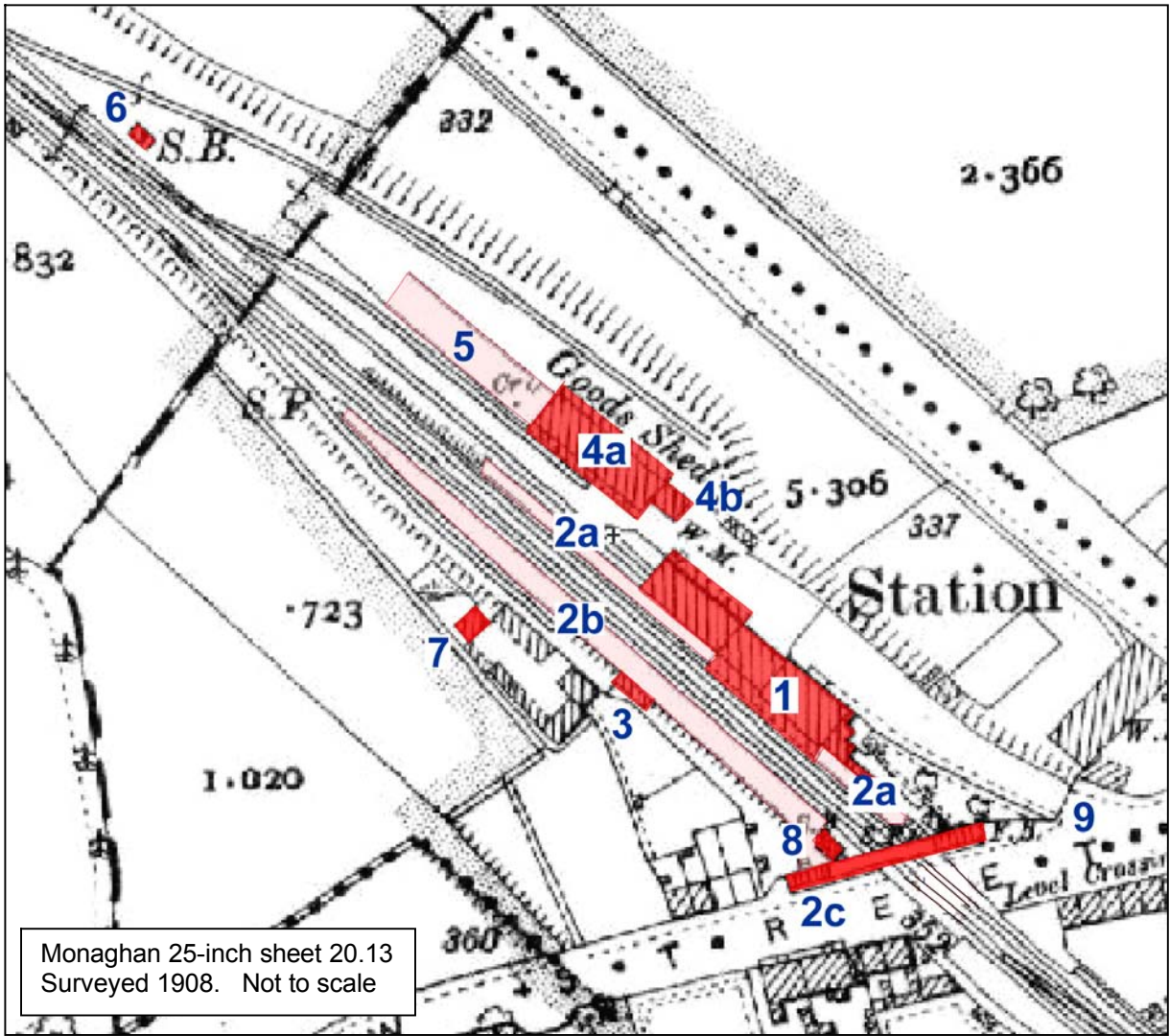
03501_04 John Langford Source Ferris 2003, p.96.

Castleblaney signal cabin. The spur at right formerly went to Armagh and was used as a siding after the closure of this line.

03501_05 Ian Allan Library 31/07/1958 Source Ferris 2003, p.96.
Boxes of fish being unloaded from railcar C's trailer at Castleblaney Station.

03501_06 John Langford 31/12/1959 Source Johnston 2005, p.100.
Signal cabin at Castleblaney Station.

Component	03501	01	Type	Station building; Station house		
County	Monaghan		OS 6-inch map	MO020	Grid	282390 319750
Surveyed?	Y		Survey date	29/10/2007	Surveyor	AR&R
Completeness	Substantial remai		Condition	Good	Use	Dwelling
Description	<p>Detached U-plan three-bay two-storey former station building, built c.1849, with modern single-storey extension to SW rear (platform) elevation. Originally accommodated ticket office, waiting rooms and toilets station along with station master's living quarters. Now converted into flats and in use as multiple dwelling. Single-storey section of station building demolished but scar still visible on NW gable. Hipped slate roof with terracotta ridge tiles and yellow brick chimneystacks with moulded brick cornice. Projecting eaves supported by paired timber brackets. Rendered walls with ruled and lined detail. Painted sandstone plinth course visible on SW elevation. Square-headed window openings with sandstone sills and six-over-six timber sash and replacement uPVC windows. Two square-headed door openings on NE elevation with glazed overlights.</p> <p>Single-storey section of station building demolished but scar still visible on NW gable. Mark of long-removed platform canopy also visible at first floor level on platform side.</p>					
Interest	Architectural; Historical; Social; Group					
Evaluation	<p>Although no longer in use as a station, the form and plan of this building make it recognisable as such. It retains some original features such as timber sash windows, doors, polychromatic chimneystacks and roof all of which contribute to its architectural importance, despite the removal of the single-storey annexe. Unfortunately none of its associated structures survives. The building is also of historical and social interest and is of regional industrial heritage significance. It merits inclusion on the Record of Protected Structures.</p>					
Significance	Regional			Action RPS		



Component	03501 02a	Type	Platform		
County	Monaghan	OS 6-inch map	MO020	Grid	282373 319756
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	This was the 'up' platform, of which nothing survives. Old photos show a water pipe with attached hose (to refill boilers).				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03501 02b	Type	Platform		
County	Monaghan	OS 6-inch map	MO020	Grid	282359 319754
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing survives of the 'down' platform.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03501 02c	Type	Bridge (foot/rail)		
County	Monaghan	OS 6-inch map	MO020	Grid	282418 319710
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Site of footbridge over lines at road end of complex.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03501 03	Type	Waiting shelter			
County	Monaghan	OS 6-inch map	MO020	Grid	282365	319742
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	New houses on site. Old photos show a single-storey monopitched roof building.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03501 04a	Type	Goods shed			
County	Monaghan	OS 6-inch map	MO020	Grid	282358	319792
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	An open grassy area marks the site of the former goods shed.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03501 04b	Type	Goods office/ weighbridge			
County	Monaghan	OS 6-inch map	MO020	Grid	282372	319782
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of this office and weighbridge are gone.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03501 05	Type	Goods beach			
County	Monaghan	OS 6-inch map	MO020	Grid	282333	319812
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	There was a crane on this platform, no trace of which survives. .					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03501 06	Type	Signal box			
County	Monaghan	OS 6-inch map	MO020	Grid	282268	319856
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing remains of Castleblaney West cabin.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03501 07	Type	Water tank			
County	Monaghan	OS 6-inch map	MO020	Grid	282331	319757
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing remains of this water tank or its base.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03501 08	Type	Signal box		
County	Monaghan	OS 6-inch map	MO020	Grid	282404 319712
Surveyed?	Y	Survey date	02/07/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing remains of Castleblaney East cabin.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03501 09	Type	Entrance (station)		
County	Monaghan	OS 6-inch map	MO020	Grid	282452 319720
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R
Completeness	Substantial remain	Condition	Good	Use	Entrance
Description	Tall square-profile cut-stone entrance piers at entrance to former railway complex survive. Modern replica gates to piers. Pedestrian entrance adjoins.				
Interest	Architectural; Historical; Group; Setting				
Evaluation	These gate pillars have group value with the station building and are prominent at its entrance. They are of regional industrial heritage significance, meriting inclusion on the Record of Protected Structures.				
Significance	Regional			Action	RPS

03501_01 FWH 02/07/2007 Station building from east.



03501_02 AR&R 02/07/2007 Front (NE) elevation of station building.



03501_03 AR&R 02/07/2007 Rear (SW) elevation of station building.



03501_04 AR&R 02/07/2007 Eaves detail.



03501_05 AR&R 02/07/2007 Detail of yellow brick chimneystacks.



03501_06 AR&R 02/07/2007 Painted stone plinth course on SW elevation.



03501_07 AR&R 02/07/2007 Detail of window.



03501_08 AR&R 02/07/2007 Detail of window.



03501_09 FWH 02/07/2007 Entrance to station from SE.



Site 03502

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03502 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	282200	319896
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This area has been redeveloped as an industrial estate and there is now no trace of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03503

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.
This bridge may be the one cited by Mills (1903) as having had a replacement deck in the period 1877-1903.

Component	03503 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO020	Grid	281958	320089
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This is now the entrance to Roadstone's Castleblaney Quarry. The bridge carrying the railway over the accommodation road has been removed. The embankment has been removed to the SE of the bridge. It survives at SW, shored up with a modern concrete wall.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03504

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03504 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	281807	320178
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This area has been quarried away.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03505

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03505 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO020	Grid	281661	320238
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This accommodation bridge over the railway cutting now lies within Roadstone's Castleblaney quarry and has been demolished.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03506

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03506 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	281466	320275
Surveyed?	N					

Site 03507

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03507 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO020	Grid	281193	320245
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	The bridge on which the railway crossed an accommodation track and its associated embankment have both been removed.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03508

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03508 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO020	Grid	281098	320213
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Disused	

Description This arched bridge carries the single-line railway over an accommodation track. The abutments, which also incorporate the embankment wing walls on one side, are of squared random rubble and have dressed quoins. The arch is of segmental profile and has dressed stone voussoirs; its span is estimated at 3.4-3.7m (11-12ft). The spandrels are of random rubble and are coped with chamfered masonry blocks. Angled wing walls slope down on each side; these are detailed and coped as the spandrels. Boxes have been erected underneath the arch to encourage nesting birds. A short section of steep embankment survives at both ends of the bridge.

Interest Architectural; Historical; Setting

Evaluation This well executed bridge, which is clearly visible from the road, is of more than passing interest because of its arch being narrower than the abutments. It also has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03508_01 FWH 24/07/2007 View of bridge from north.



03508_02

FWH

24/07/2007

South elevation from SW.



Site 03509

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03509 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	280816	320158
Surveyed?	Y	Survey date	24/07/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	Two cylindrical pillars survive on the south side of the former line and one on the north. The are of random rubble construction and coped with concrete. The SE pillar has been rebuilt. No gates survive.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03509_01 FWH 24/07/2007 General view of gate pillars from south.



Site 03510

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03510 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	280727	320163
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces survive. The new Castleblaney bypass crosses hereabouts.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03511

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03511 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	280568	320195
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate and its two cylindrical pillars survive on the south side an accommodation crossing over the former track, together with a single pillar on the north side. The pillars are of random rubble and are coped with bricks and concrete.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03511_01 FWH 17/08/2007 South gate and pillars, from north.



Site 03512

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03512 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO020	Grid	280346	320283
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Fair	Use	Disused	
Description	A single cylindrical gate pillar survives on the south side of an accommodation crossing over the line. It is of random rubble construction.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Site 03513

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03513 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO020	Grid	280252	320335
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This accommodation bridge carried a track over a cutting, now flooded and heavily overgrown. According to a local informant, it was a beam bridge and is possibly buried rather than demolished.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03514

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Cited as wooden bridge on 2nd edition OS map. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03514 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO019	Grid	280011	320472
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No trace of this accommodation bridge over a cutting survives. The latter has been partly infilled.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03515

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03515 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	279693	320646
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate and its two cylindrical pillars survive on the south side of an accommodation crossing, together with a gate and single pillar on the north side. The pillars are of concrete-rendered random rubble and brick. The missing gate post on the north side was removed to facilitate modern farm machinery. The surviving gates are in poor condition.					
Interest	None					
Evaluation	These gates are of local industrial heritage significance because of their relative completeness and historical association with the railway.					
Significance	Local					

03515_01 FWH 17/08/2007 South gate and pillars, from north.



Site 03516

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03516 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO019	Grid	279393 320730
Surveyed?	N				

Description There was no access to this area (turkey farm).

Site 03517

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03517 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO019	Grid	279288	320741
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	All traces of this rail bridge over a road have been removed, together with the embankment to its west. The road has been realigned and now bypasses the site of this bridge.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03518

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03518 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO019	Grid	279060	320771
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Poor	Use	Disused	

Description An arched bridge carries a now-disused section of road over the former railway, here in a cutting. The latter has been infilled, leaving only the upper half of the east elevation visible. The abutments are of random rubble and 8.40m deep. A chamfered string course runs through the arch at spring level and around the quoins. The arch is of segmental profile and spans 9.10m. The soffit is of random rubble and the voussoirs of rusticated masonry blocks with a margined edge to the arch. The spandrels and parapets are of random rubble. A string course runs along the base of the parapets. The latter are coped with dressed stone blocks and terminate in out-projecting rusticated piers. The middle section of the east parapet has collapsed. The road approaches were dog-legged originally. A new section of road was laid immediately west of the bridge to remove this impedance.

Interest Architectural; Historical

Evaluation This bridge's architectural merit has been diminished by infilling and although of historical interest due to its railway association, is now only of local industrial heritage significance.

Significance Local

03518_01 FWH 17/08/2007 East elevation.



Site 03519

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03519 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO019	Grid	278572	320978
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	
Description	A culvert carries a stream under the former embanked railway. Heavy overgrown precluded detailed examination or photography. The arch is of semicircular profile and c.1.5m span. It has dressed stone voussoirs. The embankment has been lowered but is still in use as a field accommodation track.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Site 03520

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03520 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO019	Grid	278537	321002
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched bridge carries the former embanked line over an accommodation track. The abutments are of random rubble, brought to courses. They are continuous with the wing walls on the NE side, and have rusticated and margined quoins at the SW end. They are 5.03m deep under the arch. The latter is of segmental profile and spans 3.66m. It has a random rubble soffit and rusticated voussoirs with a margined arch edge. The spandrels are of random rubble with an oversailing dressed stone coping. The wing walls are also of random rubble. Those at NE have rusticated and margined quoins; all are coped with dressed stone blocks. The embankment has been removed to both sides of the bridge to reveal three rubble stone buttresses.

Interest Architectural; Historical

Evaluation The removal of the embankment has left this bridge as an incongruous feature of the landscape. It is also of historical interest due to its association with the railway. Overall, it is of local industrial heritage significance.

Significance Local

03520_01 FWH 17/08/2007 North-east elevation.



03520_02 FWH 17/08/2007 South-west elevation.



03520_03 FWH 17/08/2007 South-east side of bridge (embankment removed).



Site 03521

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03521 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	278317	321140
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	A cylindrical random rubble pillar survives on the north side of the former track.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Site 03522

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Deck replaced in 1955 by Great Northern Railway Board. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03522 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO019	Grid	277691	321161
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description A twin-span concrete beam bridge carries an accommodation track over the former railway, here in a cutting. The abutments are of random rubble with rusticated and margined quoins; they are 9.05m apart. The original deck has been replaced with pre-cast reinforced-concrete slabs. One end of each slab rests on a concrete pillow block on top of the abutment, and the other end on a 70cm thick concrete pier rising from the bed of the cutting. The south span measures 5.43m and the north one 2.92m. The former is slightly thicker than the latter; cast into both its edges is "GNR 1955". The deck is 4.45m wide and has a post and wire railing (with tubular steel top rail) along both sides. There are angled random rubble wing walls to each end with dressed stone copings and rusticated terminal piers. A wooden telegraph pole survives at the SW end of the bridge, on which are 21 insulators.

Interest Architectural; Historical; Technical

Evaluation This bridge is of architectural interest in being one of the few reinforced-concrete bridges on this section of line. It is of historical interest as it reflects its upgrading in the mid 1950s by the Great Northern Railway Board (created in 1953). It is of local industrial heritage significance.

Significance Local

03522_01 FWH 17/08/2007 East elevation.



03522_02 FWH 17/08/2007 West elevation.



03522_03 FWH 17/08/2007 'GNR 1955' inscription (east elevation).



Site 03523

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03523 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO019	Grid	277526	321121
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description A highly skew arched culvert carries a stream under the embanked railway. This is a mid 20th century replacement of the original culvert. The abutments are of mass concrete and 9.36m deep. The arch is of semicircular profile; it spans 1.82m orthogonally and 2.60m on the skew. The arch ring is of mass concrete, thus enabling the brick soffit to be laid orthogonally. The spandrels and parapet are also of mass concrete. The north parapet has been removed, probably when a pre-cast reinforced-concrete box culvert was added at this end to facilitate access along the bottom of the embankment. The embankment has been removed to the east of the culvert.

Interest Historical

Evaluation This structure is of historical interest because of its railway association and also because it reflects an upgrading of the line, probably in the 1950s (at the same time as the accommodation bridge to its east (3522) was upgraded. It is of local industrial heritage significance.

Significance Local

03523_01 FWH 17/08/2007 Downstream (south) elevation.



Site 03524

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03524 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	277456	321105
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site **03525**

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03525 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	277137	321068
Surveyed?	N					

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Crossing used as a railcar stop from c.1935 to 1940s. The siding (component 4) is reported as being in use c.1939. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03526 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	276646	321127
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	A timber gate post survives on the SW side of this road crossing, at the entrance to the former level-crossing keeper's house.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03526 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO019	Grid	276630	321138
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A refurbished and extended two-storey former level crossing keeper's house aligned east-west on the SW side of the road crossing. Gabled natural slate roof, with central brick chimney and uPVC eaves and bargeboards. Cement-rendered walls. Square-headed openings, with replacement frames; all the windows have concrete cills. A two-storey flat-roofed return has been added to the west end of the south elevation, across the former track bed.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03526 03	Type	Hardstand
County	Monaghan	OS 6-inch map	MO019 Grid 276661 321131
Surveyed?	Y	Survey date	17/08/2007 Surveyor Fred Hamond
Completeness	No visible remain	Condition	N/A Use N/A
Description	There was formerly a hardstand at the NE side of the road crossing, no traces of which survive.		
Interest	None		
Evaluation	No industrial heritage significance.		
Significance	Record only		

Component	03526 04	Type	Siding
County	Monaghan	OS 6-inch map	MO019 Grid 276679 321132
Surveyed?	Y	Survey date	17/08/2007 Surveyor Fred Hamond
Completeness	No visible remain	Condition	N/A Use N/A
Description	There was a siding on the north side of the line, to the NE of the road crossing. Now heavily overgrown, there are no visible traces save for a timber palisade along the road, of vertical timber sleepers.		
Interest	None		
Evaluation	No industrial heritage significance.		
Significance	Record only		

03526_01 FWH 17/08/2007 Refurbished house from east. Note also gate post.



Site 03527

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03527 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	276521	321144
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing remains of this accommodation crossing.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03528

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03528 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	276186	321189
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate between two cylindrical posts survives on the south side of the line, together with two posts on the north side. The posts are of cement-rendered random rubble.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03528_01 FWH 17/08/2007 Gate and posts from south.



Site 03529

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03529 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO019	Grid	276056	321210
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description A skew arch span carries the former railway over a stream. The abutments are of random rubble, with dressed and margined quoins. A chamfered stone string course runs through the arch at spring level (but not around the quoins). The arch is of segmental profile and spans 5.0 - 5.5m (measured orthogonally). The soffit is of random rubble and the voussoirs of rusticated masonry blocks with a tooled margin to the arch edge. The spandrels and low parapets are of random rubble. The latter are coped with dressed stone blocks and are c.20m apart. There are curved squared random rubble wing walls at SE and NW. The former line is now an accommodation track.

Interest Architectural; Historical

Evaluation Although utilitarian architecturally, this is a substantial bridge on account of its depth. It also has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03529_01 FWH 17/08/2007 Upstream (south) elevation.



Site 03530

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03530 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO019	Grid	275512	321145
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	The curved SW wing wall is all that survives of this rail bridge over an accommodation track. It is of random rubble and has a concrete coping. The rest of the bridge and its embankment have been removed to facilitate access to fields.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03530_01 FWH 17/08/2007 Wing wall from NE.



Site 03531

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03531 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	275045	321100
Surveyed?	N					

Site 03532

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03532 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO019	Grid	274832	321046
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	An arched bridge carries the embanked railway over a stream. It is of split random rubble throughout. There is no embellishment to the abutment quoins. The arch is of semicircular profile and spans c.5.65m. It has a rubble masonry soffit and rusticated voussoirs with a margined edge to the arch. The spandrels are coped with dressed stone. There are angled wing walls to both sides. Now heavily overgrown.					
Interest	Architectural; Historical					
Evaluation	This utilitarian bridge has an historical association with the railway and is of local industrial heritage significance.					
Significance	Local					

03532_01 FWH 17/08/2007 Upstream (north) elevation.



Site 03533

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03533 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO019	Grid	274758 321020
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Poor	Use	Disused
Description	A wrought-iron sunburst gate and timber post survive on the north side of this accommodation crossing, along with two cylindrical pillars on the south side. The latter are of random rubble and coped with cement.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03533_01 FWH 17/08/2007 Gate from north.



Site 03534

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03534 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO019	Grid	274553 320927
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of this accommodation crossing survive; the track bed has been reclaimed for agriculture.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Site 03535

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03535 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	274362	320868
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate and cement-rendered random rubble cylindrical pillar survive on the south side of the accommodation crossing, along with a single pillar on the north side.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03535_01 FWH 17/08/2007 South gate and pillars, from north.



Site 03536

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03536 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	274192	320836
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive; the track bed has been reclaimed for agriculture.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03537

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Deck replaced in mid 1950s by Great Northern Railway Board. Closed to passengers by the Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03537 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO019	Grid	274024	320822
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description A twin-span concrete beam bridge carries an accommodation track over the former railway, here in a cutting. The abutments are of squared random rubble, brought to course and embellished with rusticated and margined quoins; they are 8.73m apart. The original deck has been replaced with pre-cast reinforced-concrete slabs. One end of each slab rests on a concrete pillow block on top of the abutment, and the other end on a 74cm thick concrete pier rising from the bed of the cutting. The south span measures 4.88m and the north one 3.11m; the former, now infilled, is slightly thicker than the latter; cast into both its edges is "GNR B" (Great Northern Railway Board). The deck is 4.40m wide and has a post and wire railing (with tubular steel top rail) along both sides. They terminate in coped random rubble piers; the two piers at the south end have been rendered with cement during the resurfacing of the laneway to a new house. There are angled split random rubble wing walls to each end with dressed stone copings.

Interest Architectural; Historical; Technical

Evaluation This bridge is of architectural interest in being one of the few reinforced-concrete bridges on this section of line. It is of historical interest not only on account of its railway association, but also because it demonstrates the bridge's upgrading in the mid 1950s by the Great Northern Railway Board (created in 1953). It is of local industrial heritage significance.

Significance Local

03537_01 FWH 17/08/2007 East elevation.



03537_02 FWH 17/08/2007 West elevation.



03537_03 FWH 17/08/2007 'GNR B' inscription on west edge of south span.



Site **03538**

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03538 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	273609	320836
Surveyed?	N					

Site 03539

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03539 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO019	Grid	273378	320850
Surveyed?	Y	Survey date	17/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	All that remains of this railway bridge is an overgrown truncated random rubble abutment on the east side of the road which it crosses. The road approaches were originally dog-legged, but it has since been widened and straightened.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Site 03540

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03540 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO019	Grid	273166	320865
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This section of the line has been reclaimed for housing and no traces of this bridge over an accommodation track survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03541

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03541 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	272782	320883
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site **03542** **Knocknamaddy Crossing; Knocknamaddy Halt**

History Line opened by the Dundalk & Enniskillen Railway in July 1854. Used as a railcar stop from c.1935 to 1940s. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959. The level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

Component	03542 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	272617	320853
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate lies buried in a hedge at the NE corner of this accommodation crossing. None of the posts survives.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03542 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO019	Grid	272657	320873
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	The original level crossing keeper's house was demolished to make way for the present single-storey house, known as 'Railway Lodge'.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03542 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO019	Grid	272634 320864
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of the hardstand associated with this request halt survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

An Industrial Heritage Survey of Railways in Counties Monaghan and Louth Part 2



SITE INVENTORY: VOLUME 2.2

Dundalk - Enniskillen Line (Sites 03601-03908)

for
Monaghan County Council and Louth County Council
December 2007



History

Line opened by the Dundalk & Enniskillen Railway in July 1854. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959. Used as shoe factory after closure in 1957. Interior gutted by fire c.1980. The goods shed (component 5a) was demolished c.2005.

The signal box (component 3) and station house date from the period 1877-1903 and are the work of W.H. Mills, the GNR(I)'s Chief Engineer at that time. The railway-related building (6) was built after 1908 as it is not shown on the OS 25-inch map.

03601_01 E.M. Patterson 31/07/1958 Source C. Friel Collection: EMP43S.

Ballybay Station from a Dundalk to Cavan railcar.



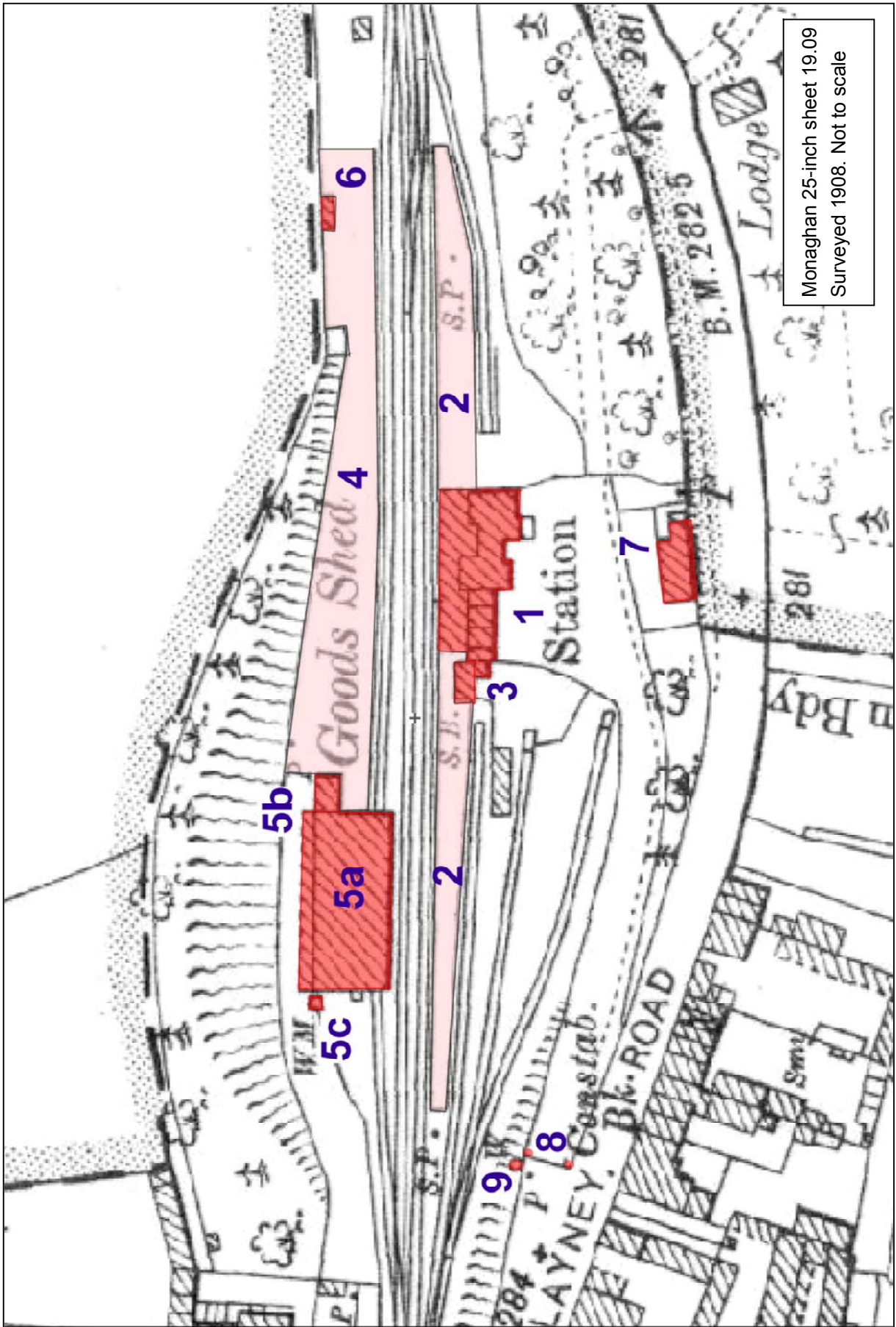
03601_02 Unknown Source C. Friel Collection: Real Photographs X92.

Ballybay Station with Carrickmacross branch train waiting to leave.

03601_03 Unknown Source Ferris 2003, p.97.

Ballybay Station. The bay at right served the Cootehill branch.

Component	03601 01	Type	Station building			
County	Monaghan	OS 6-inch map	MO019	Grid	271959	320649
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Offices	
Description	<p>Detached multiple-bay former railway station, with two-storey gabled section to west and single-storey gabled section to east, both connected by single-storey central section. Also a single storey section to west connecting with signal box to west. Incorporated ticket office, waiting rooms and parcel office. Now refurbished as offices. Flat-roofed entrance porch to south elevation replaces original gabled entrance porch. Canopy to platform elevation over platform now removed. Replacement pitched roof with fibre-cement tiles and rainwater goods. Rendered south wall with ruled and lined detail. North elevation to platform and east side elevation comprise coursed rubble stone walls with punch dressed sandstone quoins and plinth course. Carved sandstone quatrefoil to single-storey gabled section on north platform elevation.</p> <p>Pointed-arched window openings to south elevation with rendered chamfered reveals and sills and timber sash windows. Pointed-arched window openings to east side</p>					



elevation and north platform elevation with chamfered sandstone surrounds. Square-headed window opening to ground floor of gabled two-storey section has a carved sandstone label and pair of pointed-arched window openings to same section has carved sandstone hood-mouldings. A variety of timber sash and casement windows. Variations in window surrounds to north elevation suggest openings were inserted and altered over time.

Pointed-arched door opening to north platform elevation has a chamfered sandstone surround matching the window openings and a replacement timber and glazed double door. Flat-roofed entrance porch to south yard elevation is accessed up sandstone steps. Painted stone plinth course to base of porch suggests this is the original gabled entrance porch remodelled. Replacement door and windows to entrance porch.

Single-storey section at west has a pitched slate roof with terracotta ridge tiles. Rendered wall with ruled and lined detail to south yard elevation, snecked limestone wall to north platform elevation. Pointed-arched window and door openings have rendered chamfered reveals to south elevation and tooled sandstone chamfered surrounds to north elevation. Entrance porch to south elevation has a replacement door and is accessed up sandstone steps (later insertions).

No original features survive internally. The floor plan has been altered to accommodate new office use.

Interest Architectural; Historical; Social; Group

Evaluation Despite the fire c.25 years ago, the exterior of the Tudor-style structure has survived relatively intact and has a rendered south-facing elevation and exposed rubble stone north-facing elevation. Chamfered sandstone window reveals and carved hood mouldings and labels emphasise the Tudoresque style of the building, similar to the one at Newbliss. It has group value with the rest of the complex and was of social importance to the town. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Significance Regional Action RPS

Component	03601	02	Type	Platform		
County	Monaghan		OS 6-inch map	MO019	Grid	271957 320656
Surveyed?	Y		Survey date	29/10/2007	Surveyor	AR&R
Completeness	Substantial remai		Condition	Fair	Use	Disused
Description	Platform located to north of station building. A modern concrete block wall denotes its edge. Remains of terracotta tiles to surface, with textured faces to provide a non-slip surface to platform. Removed to west of signal box. Void under platform at signal box for levers. This opening is spanned by "Bridge" rails from a earlier rail track.					
Interest	Historical; Group					
Evaluation	What survives is primarily of group value, giving functional context to the station building. For this reason, it is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional				Action	RPS

Component	03601 03	Type	Signal box			
County	Monaghan	OS 6-inch map	MO019	Grid	271941	320649
Surveyed?	Y	Survey date	29/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	<p>Signal box located at end of platform to the west of the station building. Comprises a redbrick base with timber and glazed cabin above. Pitched slate roof to structure with terracotta ridge tiles. Decorative bargeboards to gable ends, timber finial survives to west gable end. Decorative brackets to eaves. Base with redbrick walls laid in English bond with rectangular recessed niches to side elevations. Segmental-headed window openings set into three of the four niches. Timber window frames survive only. Larger door opening inserted in west side elevation of brick base with steel double doors. Timber and glazed cabin above base with timber weatherboarding to south elevation. Large glazed panels to centre of north elevation flanked by horizontal sliding windows. Square-headed door opening on east side elevation has a timber and glazed door, door opening to west side elevation has a plywood door. External timber stairs to both doors now gone.</p> <p>Points rods project out the base of the west end of its north elevation and run under the platform. The 'staff instruments', which controlled these levers, were removed some years ago to a railway museum in Enniskillen.</p>					
Interest	Architectural; Historical; Group					
Evaluation	This is a good example of the many signal boxes designed by the GNR(I) engineer W.H. Mills. It also has group value in the overall station complex. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

Component	03601 04	Type	Goods beach			
County	Monaghan	OS 6-inch map	MO019	Grid	271960	320671
Surveyed?	Y	Survey date	29/10/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Poor	Use	Disused	
Description	Raised platform area survives but its edge is not longer visible. Removed at west end.					
Interest	Historical; Group					
Evaluation	To little of this platform survives to be of more than local interest due to its group value with the rest of the station.					
Significance	Local					

Component	03601 05a	Type	Goods shed			
County	Monaghan	OS 6-inch map	MO019	Grid	271904	320673
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03601 05b	Type	Goods office			
County	Monaghan	OS 6-inch map	MO019	Grid	271921	320674
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Fair	Use	Electricity substation	
Description	Single-storey former office of goods shed survives within a large modern shed. Formerly abutted east gable of now-demolished goods shed. Pitched slate roof with terracotta ridge tiles. Coursed rubble limestone walls with tooled quoins. Segmental-headed window openings with redbrick surrounds and concrete sills. East side elevation of former office is part of exterior wall of modern shed and is visible from site of former goods platform. Now houses ESB electricity supply to Leonard Engineering.					
Interest	Historical; Group					
Evaluation	Unfortunately, this office has lost its context due to the demolition of the goods shed. Although it has some group value, it is therefore of local industrial heritage significance only.					
Significance	Local					

Component	03601 05c	Type	Weighbridge			
County	Monaghan	OS 6-inch map	MO019	Grid	271883	320676
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this structure survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03601 06	Type	Railway-related building			
County	Monaghan	OS 6-inch map	MO019	Grid	272041	320672
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	Detached single-storey former railway-related structure, probably for the use of station staff and located on goods platform, east of goods shed. No longer in use. Pitched slate roof with terracotta roll-top ridge tiles and ogee cast-iron rainwater goods. Redbrick walls laid in English bond. Segmental-headed door opening to south front elevation. No door survives.					
Interest	Historical; Group					
Evaluation	This building is of local interest due to its group association with the rest of the station complex. It is of local industrial heritage interest.					
Significance	Local					

Component	03601 07	Type	Station house			
County	Monaghan	OS 6-inch map	MO019	Grid	271962	320609
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	<p>Detached three-bay two-storey former station house, with addition of porch to front elevation and modern kitchen extension built to east. Now in use as a private dwelling. Located south of station building. Rear elevation of railway house forms part of south boundary wall of railway site.</p> <p>Pitched slate roof with terracotta ridge tiles, two redbrick chimneystacks and cast-iron rainwater goods. Roof gabled over round-headed window opening to front elevation. Decorative timber fascia to front and rear elevations and pierced bargeboards with simple timber drops from apex to end gables. Decorative timber drops survive on east end gable only.</p> <p>Redbrick walls laid in English bond with yellow brick quoins, plinth course and first floor sill level course. Built on a high snecked limestone retaining wall to rear elevation.</p> <p>Segmental-headed window openings with yellow brick arches, two-over-two timber sash windows and granite sills. Round-headed window opening to first floor front elevation. Pair of narrow segmental-headed window openings to ground floor rear elevation.</p> <p>Round-headed door opening with glazed fanlight and replacement door concealed behind entrance porch (a later addition). Original timber panelled door appears to have been relocated from original position to new porch.</p> <p>Redbrick wall to east of house built originally to conceal private yard to house. Now accommodates modern kitchen extension. New square-headed window openings in wall with brick flat arches and concrete sills.</p> <p>Coursed rubble stone boundary wall to east of former station house curves noticeably and incorporates a square-profiled rock-faced pier. Possibly former entrance to railway site.</p> <p>Interior features and floor plan survive throughout house. Modern kitchen to east. Ground floor room to east retains black stone fireplace painted to resemble marble with marble panels and a cast-iron and tiled insert. Ground floor to west retains cast-iron fireplace and run-in-situ cornice. Timber panelled doors access central entrance hall from both ground floor rooms. Staircase comprises an open-string dog-leg stairs with heavy newel post at ground floor and turned banisters.</p>					
Interest	Architectural; Historical; Group					
Evaluation	<p>This house is almost identical to those at Newbliss and Carrickmacross. The use of redbrick, the segmental-headed window openings and interior joinery elements are typical of the period and are commonly found in the construction of railway complexes. This building has architectural merit, an historical association with the railway and group value in the context of the overall complex. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.</p>					
Significance	Regional			Action	RPS	

Component	03601 08	Type	Entrance (station)			
County	Monaghan	OS 6-inch map	MO019	Grid	271854	320633
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Substantial remai	Condition	Fair	Use	Disused	
Description	Principle entrance gates to station located west of the station building on Castleblaney Road. Square-profile rock-faced limestone gate piers with replacement concrete capstone. Pier to north is partially suck into (engaged) the boundary wall, pier to south is the terminating point for the boundary wall along the south side of the site. Gate piers support modern steel gates. Coursed rubble limestone wall to boundary of site. Modern rendered boundary wall built through former railway site to demark site division between Leonard Engineering and Murnaghan Brothers Ltd with sandstone coping stones.					
Interest	Historical; Group; Setting					
Evaluation	The gate pillars and boundary wall are a striking feature on the main road and have group value in the context of the overall complex. They are of regional industrial heritage significance and merit inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

Component	03601 09	Type	Well			
County	Monaghan	OS 6-inch map	MO019	Grid	271851	320638
Surveyed?	Y	Survey date	29/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	Well located set into limestone boundary wall adjacent to the principle entrance gate to the former railway site. Niche in masonry wall with cut limestone reveals to opening. Interior walls built of brick and painted. Wrought-iron gate to opening. A pump was formerly located adjacent to well (shown on OS map).					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03601_01 AR&R 29/10/2007 General view from SW.



03601_02 FWH 25/06/2007 General view of site from NW.



03601_03 AR&R 29/10/2007 Front (south) elevation of station building.



03601_04 FWH 25/06/2007 Station building from NE.



03601_05 AR&R 29/10/2007 Entrance to former station building.



03601_06 AR&R 29/10/2007 Extension to west of station building.



03601_07 AR&R 29/10/2007 North elevation of extension to west.



03601_08 AR&R 29/10/2007 Door to north elevation.



03601_09 AR&R 29/10/2007 East elevation.



03601_10 AR&R 29/10/2007 Detail of painted stone plinth course.



03601_11 AR&R 29/10/2007 Quatrefoil to north elevation of single-storey gable.



03601_12 AR&R 29/10/2007 Sandstone quins.



03601_13 AR&R 29/10/2007 Window openings to two-storey gabled north elevation.



03601_14 AR&R 29/10/2007 Window to north elevation.



03601_15 AR&R 29/10/2007 Windows to north elevation.



03601_16 AR&R 29/10/2007 Window to south elevation.



03601_17 AR&R 29/10/2007 Platform area with concrete wall along edge.



03601_18 AR&R 29/10/2007 Platform area to north of station building.



03601_19 AR&R 29/10/2007 Detail of terracotta tiled surface to platform.



03601_20 AR&R 29/10/2007 South elevation of signal box.



03601_21 FWH 25/06/2007 Signal box from SW



03601_22 AR&R 29/10/2007 East gable of signal box.



03601_23 AR&R 29/10/2007 North elevation of signal box.



03601_24 AR&R 29/10/2007 Door opening on west gable of signal box.



03601_25 AR&R 29/10/2007 Detail of timber brackets to roof of signal box.



03601_26 AR&R 29/10/2007 Detail of redbrick base to signal box.



03601_27 FWH 25/06/2007 "Barlow"-type rails over void carrying signal box levers under platform.



03601_28 AR&R 29/10/2007 Goods office from east.



03601_29 AR&R 29/10/2007 Goods office: detail of window opening on east gable.



03601_30 AR&R 29/10/2007 View of goods office inside modern shed.



03601_31 AR&R 29/10/2007 Goods office.



03601_32 AR&R 29/10/2007 Detail of windows on goods office.



03601_33 FWH 25/06/2007 Station-related building, from SE.



03601_34 AR&R 29/10/2007 Detail of door opening of station-related building.



03601_35 FWH 25/06/2007 Station house from NW.



03601_36 AR&R 29/10/2007 Station house: west gable.



03601_37 AR&R 29/10/2007 Station house: south (rear) elevation.



03601_38 AR&R 29/10/2007 Station house: east gable and extension.



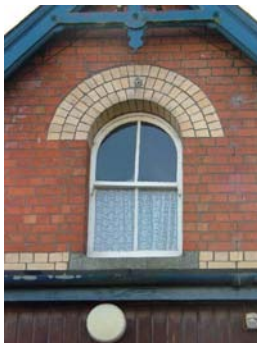
03601_39 AR&R 29/10/2007 Station house: modern kitchen extension to east.



03601_40 AR&R 29/10/2007 Ground floor window of station house.



03601_41 AR&R 29/10/2007 Detail of first floor window of station house.



03601_42 AR&R 29/10/2007 Entrance porch to station house.



03601_43 AR&R 29/10/2007 Granite sill to window of station house.



03601_44 AR&R 29/10/2007 Fireplace to ground floor room to east in station house.



03601_45 AR&R 29/10/2007 Fireplace to ground floor room to west in station house.



03601_46 AR&R 29/10/2007 Station house: staircase.



03601_47 AR&R 29/10/2007 Station house: staircase detail.



03601_48 AR&R 29/10/2007 Station house: cornice to ground floor room to east.



03601_49 AR&R 29/10/2007 Station house: door to ground floor room to east.



03601_50 AR&R 29/10/2007 General view of entrance gates.



03601_51 AR&R 29/10/2007 Entrance pier to north.



03601_52 AR&R 29/10/2007 Entrance pier to south.



03601_53 AR&R 29/10/2007 View of well and entrance adjacent.





Site 03602

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03602 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO019	Grid	271767	320659
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces survive of this metal beam bridge which carried the railway over a public road. The west abutment has been removed and the area behind it re-landscaped for public seating.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03603

History Erected by GNR(I) in 1921.

Component	03603 01	Type	Water tank			
County	Monaghan	OS 6-inch map	MO019	Grid	271685	320668
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Disused	
Description	<p>A concrete water tower over a two-bay, two-stage brick tower on the left (east) bank of the Dromore River, at the bottom of the embankment immediately NE of the railway bridge (03604). An inclined metal ladder on the south side of the building leads to the top of the open tank. Cast into the south side of the tank is '1921', indicating its construction date. There is also a depth gauge, calibrated in feet, on this side. The walls are of unrendered brick. All openings have semicircular heads. There are doorways at ground level on both the east and west elevations, with spoke-headed windows over. There are two spoke-headed windows to the north and south elevations of the ground and first stages (one per bay); all the windows have flush concrete cills.</p> <p>Internally, the building is divided into two bays. The floor is of concrete and the walls are unrendered. The ceiling is of reinforced concrete. There was originally an engine and pump in the west bay, long removed. The ladders to the upper stage have been removed. Cast-iron water pipes rise to the upper stage in both bays.</p> <p>The water intake pipe survives on the left bank of the river, just above the bridge. Concrete steps lead up to the top of the railway embankment where there are the remains of a supporting metal frame for a water hose which filled the locos' tenders.</p>					
Interest	Architectural; Historical; Group; Rarity					
Evaluation	This is an imposing building which has an historical association with the railway. It is markedly different to the usual cast-iron tank over a masonry towers. It also forms a group with the railway embankment and bridge. It is of regional significance and merits its current inclusion in the Record of Protected Structures.					
Significance	Regional	Protection	Monaghan RPS Local 51			

03603_01 FWH 20/09/2007 Tower and water hose remains from east.



03603_02 FWH 20/09/2007 Tower and water hose remains from SE.



03603_03 FWH 20/09/2007 Date and water level gauge on side of tower.



03603_04 FWH 20/09/2007 West bay of ground floor, looking north.



03603_05 FWH 20/09/2007 Water intake pipe.

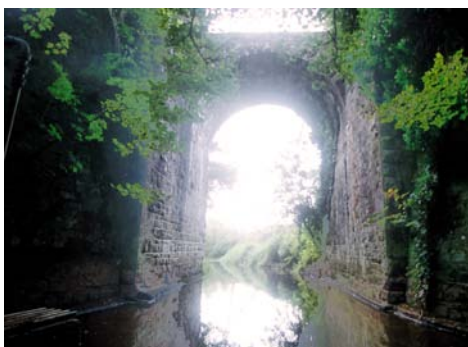


Site 03604

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03604 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO019	Grid	271678	320657
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Foot bridge	
Description	An arched masonry bridge carries the embanked line over the Dromore River. The abutments are of random rubble and are embellished with rusticated and margined quoins and incorporates the embankment wing walls on the north side. The sections which are not arched over is coped with concrete. There are also concrete footings to the abutments. The arch utilizes only the southern half of the abutments and is of semicircular profile; it spans 5.97m. Its soffit is of squared random rubble and it is embellished with rusticated voussoirs; there is a margined edge to the arch. Angled random rubble wing walls project at both ends; they are coped with rusticated and margined stone blocks. The SW wing wall is curved. The spandrels are of random rubble. The parapet comprises a four-bar tubular-steel railing and cast-iron posts mounted on a dwarf concrete wall over the spandrels. The walls are 4.40m apart. The line is now an unofficial footpath hereabouts.					
Interest	Architectural; Historical; Group					
Evaluation	This is a relatively tall bridge with an historical association with the railway and group value with the adjoining water tower to its north-east. It is of regional significance and merits its current inclusion in the Record of Protected Structures (Ballybay no.50).					
Significance	Regional	Protection	Monaghan RPS Local 50			

03604_01 FWH 20/09/2007 North elevation.



03604_02 FWH 20/09/2007 South elevation.



03604_03 FWH 20/09/2007 Looking east across deck of bridge.



Site 03605

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03605 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO019	Grid	270827	320712
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03606

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03606 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	270002	321105
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched masonry bridge carries the embanked line over an accommodation track. The abutments, which are 4.90m deep, are of random rubble, brought to courses and embellished with rusticated and margined quoins. The arch is of segmental profile and spans 3.67m. It has a rubble masonry soffit and rusticated voussoirs, with a margined edge to the arch. The spandrels are of squared random rubble, with rusticated and margined copings. There are angled wing walls to each side; they are of squared rubble brought to courses and coped as the spandrels.

Interest Architectural; Historical

Evaluation This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03606_01 FWH 20/09/2007 North elevation.



Site 03607

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03607 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO018	Grid	269865	321206
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	An arched masonry bridge carries the embanked line over the Dromore River. The abutments are of split random rubble, brought to courses and embellished with rusticated and margined quoins. It incorporates the embankment wing walls on one side. The arch is of semicircular profile and spans 6.03m. It utilizes only the south half of the abutments and is 5.06m deep. It has a rubble masonry soffit and rusticated voussoirs, with a margined edge to the arch. The spandrels are of random rubble; dense overgrowth obscures its copings. There are curved random rubble wing walls on the north side.					
Interest	Architectural; Historical					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

03607_01 FWH 20/09/2007 Upstream (south) elevation.



Site 03608

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959. According to Mills (1903), a bridge carrying the line over a public road at Shantonagh Junction was renewed in the period 1877-1903. It may well be this bridge which he cites and probably refers to the replacement of the original timber beam deck with a lattice girder span.

Component	03608 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	269824	321231
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	

Description This skew beam bridge formerly carried the embanked line over a public road. The deck has been lifted. The abutments are of split random rubble, brought to courses and embellished with rusticated and margined quoins. They incorporate the embankment wing walls on one side. The deck spanned the south half of the abutments and was c.6.40m deep. It spanned 7.52m measured orthogonally, and c.9.20m on the skew. There is brickwork along the tops of the abutments and also masonry pillow blocks for the beam spans. Towards the tops of both abutments are four cast-iron shoes embedded in the walls; these held diagonal braces to the underside of the deck. Over the quoins of the east abutment are brick terminal piers both with concrete copings. There are angled random rubble wing walls to each side, coped with chamfered sandstone blocks. The road approaches are dog-legged. Immediately west a skew arched mill race cuts through the wing walls and embankment. It has random rubble abutments 10.80m deep. Its arch is of semicircular profile and has rusticated voussoirs; it spans 1.89m measured orthogonally and 2.33m on the skew. To the south is a triple-span masonry arch bridge over the Dromore River.

Interest Architectural; Historical; Group

Evaluation Although the deck has been lifted, this bridge is of interest because of the presence of the cast-iron shoes for the beam supports. The mill race and nearby road/river bridge enhance its interest, making it of local industrial heritage significance.

Significance Local

03608_01 FWH 20/09/2007 Bridge remains from north; mill arch at right.



03608_02 FWH 20/09/2007 Bridge remains from south. Note mill arch at left.



03608_03 FWH 20/09/2007 East abutment.



03608_04 FWH 20/09/2007 Cast-iron shoes on east abutment.



Site **03609** **Shantonagh Halt; Shantonagh Junction**

History Junction opened 1860 by Dundalk & Enniskillen Railway to serve Cootehill branch. Captioned as Ballybay Junction on 2nd edition OS map. Used as a railcar stop from c.1935 to 1940s.

03609_01 Drew Donaldson 03/06/1956 Source Patterson 2003, p.56.

U class 4-4-0 No.202 'Louth' passes the signal cabin at Shantonagh Junction with a GAA special going west.



03609_02 E.M Patterson 31/07/1958 Source C. Friel Collection: EMP44B.

Looking west to Shantonagh Junction just after the lifting of the Cootehill branch line at left; the Enniskillen line is at right.



Component	03609 01	Type	Junction		
County	Monaghan	OS 6-inch map	MO018	Grid	269781 321256
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	All traces of the junction have been cleared.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03609 02	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO018	Grid	269803 321253
Surveyed?	Y	Survey date	20/09/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of the hardstand associated with this request halt survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Site 03610

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03610 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO018	Grid	269497	321394
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description A skew arch masonry bridge carries the embanked railway over the Dromore River. The abutments are of random rubble and embellished with rusticated and margined quoins; they are 9.24m deep and incorporate the embankment wing walls on one side. The arch utilizes only the southern half of the abutments and is 5.53m deep. It is of semicircular profile, spanning 6.07m measured orthogonally/ 6.37m on the skew. Its soffit is of squared random rubble, laid orthogonally. It is embellished with rusticated voussoirs, with a margined edge to the arch. The spandrels were too overgrown to describe, but they are coped with masonry blocks. Angled wing walls project from both sides. They are of random rubble and are coped with dressed stone blocks.

Interest Architectural; Historical

Evaluation This substantial bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03610_01 FWH 21/09/2007 Downstream (south) elevation.



03610_02

FWH

21/09/2007

Soffit and left-bank (east) abutment.



Site 03611

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03611 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	269211	321518
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Fair	Use	Disused	
Description	Three stone gateposts survive - two on the south side of the line and one on the north side. They are 30-38cm square. New galvanised steel gates are now hung from two of the posts.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03611_01 FWH 21/09/2007 Crossing from south.



Site 03612

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03612 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	268540	321807
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	An arched masonry bridge carries the embanked railway over a road. The abutments are of random rubble, brought to courses and embellished with rusticated and margined quoins; they are 8.73m deep and incorporate the embankment wing walls at one side. The arch utilizes only the southern half of the abutments and is 5.05m deep. It is of shallow segmental profile and spans 7.67m. Its soffit is of random rubble and its voussoirs are rusticated, with a margined edge to the arch. The spandrels are of squared random rubble, with a dressed stone coping. Angled wing walls project from both sides. They are of random rubble and are coped with dressed stone blocks. The road approaches are dog-legged.					
Interest	Architectural; Historical; Setting					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is also a prominent feature along the road and is of regional industrial heritage significance, meriting inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03612_01 FWH 21/09/2007 North elevation.



03612_02

FWH

21/09/2007

Stonework detailing at SE end.



Site 03613

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03613 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO018	Grid	268159	322044
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	

Description This beam bridge formerly carried an accommodation track over the railway, here in a cutting. The deck has been removed. The abutments are of random rubble, with rusticated and margined quoins. Above a string course around the abutment tops are similarly detailed approach walls, the ends of which are also rusticated and margined. Access to this bridge was precluded by very dense vegetation overgrowth. Approximately 100m SW of the bridge, on the SW side of the cutting, is a wooden telegraph pole with four horizontal bars supporting 14 insulators.

Interest None

Evaluation No special industrial heritage significance.

Significance Record only

03613_01 FWH 21/09/2007 Bridge remains from east.



Site 03614

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03614 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	268002	322240
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	A wrought-iron sunburst gate and gatepost survive on the SW side of this accommodation crossing. There are also two stone posts on the other side, without a gate. A square piece of metal attached to the top of the gate near its hinge doubtless once held a 'no trespassing' sign.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03614_01 FWH 21/09/2007 Surviving gate from west.



03614_02 FWH 21/09/2007 Plate on gate which formerly carried 'no trespassing' sign.



Site 03615

History Line opened by the Dundalk & Enniskillen Railway in August 1855. The original bridge appears to have been replaced by the present culvert (component 2). Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03615 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	267920	322404
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This railway bridge over an accommodation track is shown on the OS six-inch map, but has been replaced by a drainage culvert (component 2).					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03615 02	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO018	Grid	267921	322405
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This concrete culvert conveys a drainage channel under the embanked railway. It replaces a rail/road bridge (component 1), possibly after the railway was closed. It is of mass concrete construction throughout. Its arch is of semicircular profile and is estimated to be 1.2-1.5m in span. Immediately SW, on the SW side of the track is a wooden telegraph pole with four horizontal bars carrying 14 insulators.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Local					

03615_01 FWH 21/09/2007 North-east elevation of culvert.



03615_02 FWH 21/09/2007 Telegraph pole, from SE.



Site 03616

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03616 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO018	Grid	267856	322559
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	<p>This metal girder bridge carries a disused accommodation track over the line, here in a cutting. The original deck appears to have been replaced, probably in the mid 1900s. The NE abutment is of random rubble masonry, brought to courses and with rusticated and margined quoins; it is now in very poor structural condition. The west abutment has been rebuilt in mass concrete and rendered with cement. The abutments are 5.09m deep. There are tapered random rubble buttresses to each side, with rusticated and margined quoins. At the sides of each abutment is a rusticated and margined string course which runs along the base of the approach walls. These walls are detailed as the abutments. The deck comprises two riveted metal plate girders of fish belly profile. They measure 72cm x 30cm in cross-section at centre, and 43cm x 30cm at their ends. They have a clear span of 8.78m and are set 3.98m apart. Set across their bottom flanges are steel I beams of 30cm x 15cm section. A concrete deck has been cast over these beams. The parapets comprise four-bar wrought-iron railings.</p>					
Interest	Architectural; Historical; Technical; Rarity					
Evaluation	This bridge is of architectural interest on account of its fish belly girder deck. It also has an historical association with the railway and is of local industrial heritage significance. Scale drawings are recommended by way of a permanent record.					
Significance	Local			Action	Measured drawings	

03616_01 FWH 21/09/2007 North-west elevation.



03616_02 FWH 21/09/2007 South-east elevation.



03616_03 FWH 21/09/2007 Underside of deck.



03616_04 FWH 21/09/2007 Looking NE across deck.



History Line opened by the Dundalk & Enniskillen Railway in August 1855. The original deck was probably renewed with plate girder spans on cylindrical piers in the 1920s. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03617 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	267718	322767
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	
Description	This triple-span metal beam bridge carried the embanked line over a public road and drainage channels on either side of the road. The deck was lifted when the line closed and only its supporting columns survive. These comprise four pairs of 2.0m diameter cast-iron columns, set 5.37m apart (centre to centre) and filled with concrete. The end sets of columns are sunk into the ends of the embankments. The second pair of columns from the SE end are braced to each other with horizontal and diagonal metal I girders. The third pair of columns from this end was blown up (supposedly to see what was inside!). Their remains reveal the columns to comprise 35mm thick cast-iron. The tops of the second and third columns are fluted.					
Interest	Historical; Technical; Setting					
Evaluation	Although the deck of this bridge is missing, the surviving columns are an interesting landscape feature and a reminder of this triple-span bridge's former presence. It also has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

03617_01 FWH 21/09/2007 Looking SE across remains of viaduct.



03617_02 FWH 21/09/2007 Braced columns on east side of road, from SW.



03617_03 FWH 21/09/2007 Demolished columns at west side of road.



Site 03618

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03618 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO018	Grid	267567	322914
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	This beam bridge carried an accommodation track over the line, here in a cutting. The deck has been lifted. The abutments, which are 4.44m deep, are of random rubble, brought to courses and embellished with rusticated and margined quoins. There are similarly detailed tapered buttresses along each side. Along the tops of these buttresses and around the abutment quoins is a rusticated and margined string course. Over these courses are approach walls, also detailed as the abutments. The deck had a clear span of 9.20m. Concrete block agricultural sheds have been built inside each approach.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03618_01 FWH 21/09/2007 Bridge remains from SE.



Site 03619

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03619 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	267457	322984
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	Two wrought-iron sunburst gates survive, one on each side of this accommodation crossing. The one at south retains both its gateposts, but one is missing on the north side. The SW gate post has a small plaque set into top of its rail face which reads: "GNR(I)/ Notice/ Trespasses on the line/ will be prosecuted". Vertical timber fencing survives to either side of both gates. Approximately 100m ENE of the crossing is a wooden telegraph pole with four horizontal bars and 14 insulators.					
Interest	Architectural; Historical					
Evaluation	This crossing has some architectural interest on account of its relative completeness and gatepost plaque. It also has an historical association with the railway and is of local industrial heritage significance.					
Significance	Local					

03619_01 FWH 21/09/2007 South gate, from north.



03619_02 FWH

21/09/2007 Plaque on SW gatepost.



Site 03620

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03620 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	267298	323057
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	
Description	This beam bridge carried an accommodation track over the line, here in a cutting. The deck has been lifted. The abutments, which are 4.26m deep, are of random rubble, brought to courses and embellished with rusticated and margined quoins. There are wing walls at each end, similarly detailed and coped with concrete. The deck had a clear span of 2.82m.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03620_01 FWH 21/09/2007 Abutment remains, from south.



Site 03621

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03621 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO018	Grid	267067 323106
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Fair	Use	Disused
Description	A wrought-iron sunburst gate and stone post survive on the north side of this accommodation crossing; the latter is braced with a diagonal strut. Its other post lies on the ground beside it. A gate post also survives at the SW corner of the crossing.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03621_01 FWH 21/09/2007 North gate, from east.



Site 03622

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03622 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	267019	323111
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No trace of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03623

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03623 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO018	Grid	266857	323121
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	
Description	This beam bridge carried an accommodation track over the line, here in a deep cutting. The deck has been lifted. The bridge was so densely overgrown that it could not be accessed but what is visible appears to be similar to bridge 3620. The parapets are set 4.40m apart.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03623_01 FWH 21/09/2007 North end of bridge approaches, from NW.



Site 03624

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03624 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	266620	323142
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	Two wrought-iron sunburst gates and the stone posts from which they are hung survive, one at each end of this accommodation crossing. On the south gate a square metal plaque secured with wire formerly carried a 'no trespassing' sign. The posts are 30cm square and the gates measure 3.05m long by 1.53m high (10ft x 5ft).					
Interest	Architectural; Historical					
Evaluation	The relative completeness of these gates and their historical association with the railway make them of local industrial heritage significance.					
Significance	Local					

03624_01 FWH 21/09/2007 South gate, from west.



Site 03625

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03625 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	266423	323158
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched masonry bridge carries the embanked railway over an accommodation road. The abutments are of random rubble, brought to courses and embellished with rusticated and margined quoins; they are 4.91m deep. The arch is of segmental profile and spans 3.70m. Its soffit is of random rubble and its voussoirs are rusticated, with a margined edge to the arch. The spandrels are of squared random rubble, with a rusticated and margined stone coping. Angled wing walls project from both sides. They are of random rubble and are coped with dressed stone blocks. The inside of the arch is heavily overgrown.
A short distance east of the bridge is a semicircular-arched drainage culvert.

Interest Architectural; Historical

Evaluation This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03625_01 FWH 21/09/2007 South elevation.



Site **03626** **Lislynchanan Crossing; Lislynchanan Halt**

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.
 Level crossing house (component 2) cited as watch house on 2nd edition OS six-inch map.

Component	03626 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	266144	323181
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03626 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO018	Grid	266164	323186
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	<p>A two-bay, two-storey former level crossing-keeper's house aligned east-west on the NE side of this road crossing. It has a gabled natural slate roof with central yellow-brick chimney and timber eaves and barge boards. The walls are of cement-rendered brick. All openings have square heads. The door and window frames have been replaced. The window cills are of painted concrete.</p> <p>The south elevation, facing the former railway line, has two windows to each floor. The east gable is devoid of openings. There is a single first floor window to the north elevation. The east elevation has a single bay, one-and-a-half storey return. The exposed section of the gable has a ground floor window.</p> <p>The return is detailed as the main section. It has a doorway in its south elevation and a window half way up its east gable. The north elevation of the return is blank. A new single-storey house has been built over the former track just south of the keeper's house.</p>					
Interest	Architectural; Historical					
Evaluation	This is a relatively unaltered former keeper's house with a railway association. It is of local industrial heritage significance.					
Significance	Local					

Component	03626 03	Type	Hardstand			
County	Monaghan	OS 6-inch map	MO018	Grid	266154	323185
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of the hardstand associated with this request halt survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

03626_01 FWH 21/09/2007 Level crossing house, from SW.



03626_02 FWH 21/09/2007 Level crossing house, from NW.



Site 03627

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03627 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO018	Grid	265628	323190
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	A beam bridge carried an accommodation track, now disused, over the former line, here in a cutting. The abutments are of random rubble, brought to courses and embellished with rusticated and margined quoins. They are 6.30m deep at their base and taper inwards towards the top. The span, which measures 8.17m, comprises two timber beams laid across the tops of the abutments. Between them is a riveted metal lattice girder. Across the middle of the undersides of all three beams is a timber beam. This is secured with two pairs of metal tensioning rods affixed to the tops of the abutments at the ends of the principal timber beams. Deck boards are laid across the tops of the beams, projecting over the ends; most are now missing. The handrail was probably of timber, but only its ends now survive.					
Interest	Historical; Technical; Rarity					
Evaluation	This bridge primarily of technical interest due to the construction of its beam and lattice girder deck. This is a rare surviving example. It also has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures. Measured drawings are also recommended as a permanent record.					
Significance	Regional			Action	RPS; Measured drawings	

03627_01 FWH 21/09/2007 Bridge from east.



03627_02

FWH

21/09/2007

Underside of deck.



Site 03628

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03628 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO018	Grid	265224	323176
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description A masonry arch bridge conveys the embanked railway under an unnamed stream. It is of random rubble construction throughout. The abutments have rusticated and margined quoins and have been concreted along their bases. The arch is of semicircular profile and spans c.3.50m. Its voussoirs are also rusticated, with a margined edge to the arch. Wing walls project at right angles. The spandrels and wings are coped with concrete. The embankment has been completely removed to the west of this bridge.

Interest None

Evaluation This utilitarian bridge is of local industrial heritage significance.

Significance Local

03628_01 FWH 21/09/2007 Bridge from NW.



03628_02 FWH 21/09/2007 Cut-away embankment to west of bridge, from west.



Site**03629****Monaghan Road Crossing**

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03629 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	265121	323172
Surveyed?	Y	Survey date	21/09/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History

Line opened by the Dundalk & Enniskillen Railway in August 1855. Station not opened until March 1859. According to owner, damaged by fire c.1916 and rebuilt in present form c.1920. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959. According to Mills (1903), a cottage was built hereabouts in the period 1877-1903; its precise whereabouts is uncertain.

03701_01 Drew Donaldson 12/10/1957 Source C. Friel Collection: DDNW6.

U class 4-4-0 No.201 'Meath' with the 10.30am from Dundalk at Monaghan Road on the last day of passenger services to Clones.

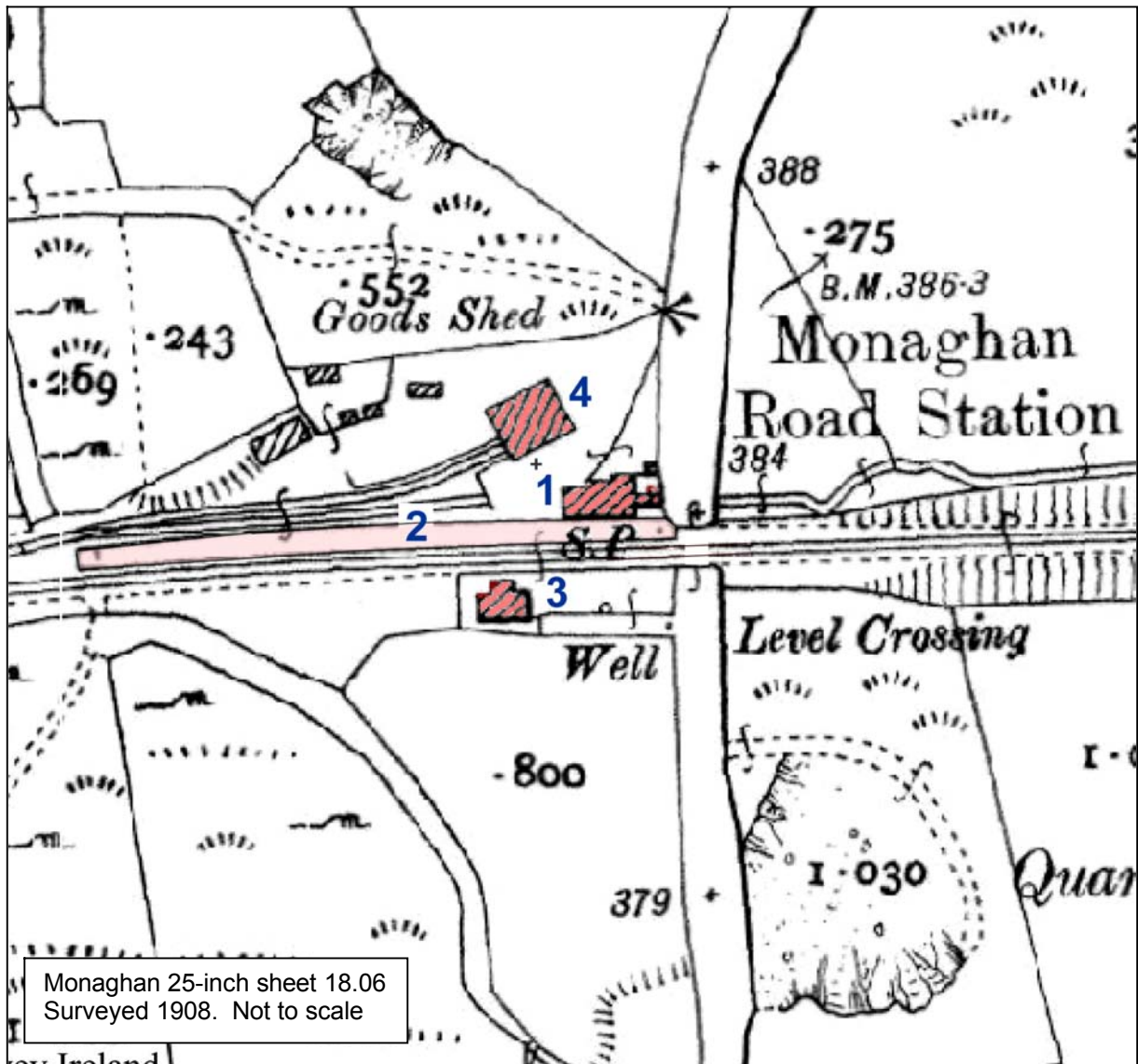


03701_02 E.M. Patterson 31/07/1958 Source C. Friel Collection: EMP44C.

Monaghan Road Station.



Component	03701 01	Type	Station building			
County	Monaghan	OS 6-inch map	MO018	Grid	265106 323183	
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Commercial premises	
Description	Detached three-bay single-storey former station building, incorporating ticket office, waiting rooms and toilets (at east end). Now in use as a furniture showroom. Pitched slate roof with terracotta ridge tiles, two redbrick chimneystacks and cast-iron rainwater goods. Roof hidden behind blocking wall to front elevation. Snecked limestone walls with sandstone quoins. Pairs of trefoil-windows with sandstone chamfered reveals flank the central entrance on platform side (the north elevation was devoid of openings). The windows have replacement timber frames. The					



doorway has a pointed-arch head with chamfered sandstone surround and carved sandstone hood-moulding (with cement repairs). Sandstone threshold to doorway and replacement timber door. Each end bay has a pair of trefoil windows with chamfered sandstone reveals. Square-headed window opening to west gable is a later addition.

Coursed rubble stone screen wall at east end of building formerly contained public toilets. These have been removed and the space covered with a flat roof to create a modern extension with pebbledash walls and flat roof.

The interior retains a few original features, notably the ticket office window, and tongue-and-groove panelling to the ceiling. Replacement cast-iron fireplaces to chimneybreasts.

Interest Architectural; Historical; Social; Setting

Evaluation This building is of architectural interest on account of its Tudor-style elements. Although small in scale and simple in its plan, it has some fine stonework to the walls and chamfered sandstone trim to the openings on the platform elevation. It is also a feature of the roadside and was also of social importance when in use. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Significance Regional Action RPS

Component	03701	02	Type	Platform			
County	Monaghan		OS 6-inch map	MO018	Grid	265083	323175
Surveyed?	Y		Survey date	31/10/2007	Surveyor	AR&R	
Completeness	Complete		Condition	Fair	Use	Disused	
Description	Coursed rubble stone platform wall with course of redbrick at top. Concrete surface to platform, evidently a later addition, extends to edge of platform over original brickwork. Some stone kerbing at west end. Concrete block wall constructed on edge of platform to west of station building.						
Interest	Historical; Group						
Evaluation	The platform has a functional association with the station building and group value. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.						
Significance	Regional				Action	RPS	

Component	03701 03	Type	Station house			
County	Monaghan	OS 6-inch map	MO018	Grid	265088	323162
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	Detached three-bay two-storey former station house. Extended to north front elevation, taking in projecting central entrance porch of original plan. Pitched roof extended over new extension. Two redbrick chimneystacks to roof and replacement rainwater goods. Rendered walls with ruled and lined detail. Segmental-headed window opening to end gable walls with painted sills and replacement timber casement windows. Square-headed window and door openings to extension.					
Interest	None					
Evaluation	Little now remains of this building's railway character, and despite its proximity to the station building, it is of no special industrial heritage significance.					
Significance	Record only					

Component	03701 04	Type	Goods shed			
County	Monaghan	OS 6-inch map	MO018	Grid	265093	323196
Surveyed?	Y	Survey date	25/06/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Furniture showroom stands on site of the demolished goods shed.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

03701_01 FWH 25/06/2007 General site view, from east.



03701_02 AR&R 31/10/2007 General view from north.



03701_03 FWH 25/06/2007 Station building and platform, from SE.



03701_04 AR&R 31/10/2007 Front (south) elevation of station building.



03701_05 AR&R 31/10/2007 West gable.



03701_06 AR&R 31/10/2007 Rear (north) elevation of station building



03701_07 AR&R 31/10/2007 Rear extension to station building.



03701_08 FWH 25/06/2007 Entrance to station building.



03701_09 AR&R 31/10/2007 Station building: detail of window.



03701_10 AR&R 31/10/2007 Station building: detail of quoins.



03701_11 AR&R 31/10/2007 Station building: detail of threshold.



03701_12 AR&R 31/10/2007 Station building: door from interior.



03701_13 AR&R 31/10/2007 Station building: ticket office hatch.



03701_14 AR&R 31/10/2007 Station building: scar of original fireplace in wall.



03701_15 AR&R 31/10/2007 Platform.



03701_16 AR&R 31/10/2007 Platform wall running west.



03701_17 AR&R 31/10/2007 Platform wall to west of station.



03701_18 FWH 25/06/2007 Station house, from NE.



03701_19 FWH 25/06/2007 Station house, from SW.



Site 03702

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03702 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	265018	323164
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03703

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03703 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	263491	323218
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03704

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03704 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO018	Grid	263308	323178
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	A beam bridge carried an accommodation track over the former line, here in a cutting. The deck has been lifted, leaving only the abutments. These are of squared random rubble, embellished with rusticated and margined quoins. They measure 4.42m deep and are 9.15m apart. On the north abutment, a rusticated and margined string course runs around the quoins and along the base of the approach wall parapets. It is missing from the south abutment.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03704_01 FWH 11/10/2007 Abutments, from west.



History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.
Level crossing house (component 2) cited as watch house on 2nd edition OS six-inch map.

Component	03705 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	262628	323075
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this road crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03705 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO018	Grid	262635	323083
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Disused	
Description	<p>A refurbished three-bay, single-storey former level crossing keeper's house aligned east-west on the NE side of the crossing. Gabled natural slate roof, with plain bargeboards, boxed eaves and brick chimney (possibly rebuilt). Walls are cement rendered. All openings have square heads. The south elevation (facing the track) has a replacement door and small window in the south bay and a window to each of the other bays. There is a window to the south gable (facing the road). The north elevation has a window to the middle bay. The east gable is blank. All the window openings have concrete cills and replacement casement frames.</p> <p>On the opposite side of the road is a small single-storey, single-bay shed with a monopitched concrete roof, red brick walls. There is a door to its road elevation and a window to the south elevation. It was probably used for storage or as a toilet originally.</p>					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03705_01 FWH 11/10/2007 Level crossing house and line of track, from SW.



03705_02 FWH 11/10/2007 Level crossing house from SE.



03705_03 FWH 11/10/2007 Shed from SE.



Site **03706** **Killygragy No.2 Crossing; Killygragy Halt**

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03706 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	262317	323027
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this road crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03706 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO018	Grid	262340	323038
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	A refurbished four-bay, single-storey former level crossing keeper's house aligned east-west on the NE side of the crossing. Gabled natural slate roof with two yellow-chimneys. Walls are pebble-dashed over brick, with vee-jointed stucco quoins. All openings have square heads. The south elevation (facing the track) has a modern single-storey hipped return on its left (west) bay. Each of the three exposed bays has a window. There is a flat-roofed modern extension to the west gable (facing the crossing) and a window to the exposed section. The north elevation has a window to the middle bay. The east gable is blank. All the window openings have thin concrete cills and replacement casement frames.					
Interest	None					
Evaluation	The refurbishment and modern additions detract from this building's architectural character to such an extent that it is no longer of any special industrial heritage significance.					
Significance	Record only					

Component	03706 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO018	Grid	262329 323030
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of the hardstand associated with this request halt survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03706_01 FWH 11/10/2007 Level crossing house from SE.



03706_02 FWH 11/10/2007 Level crossing house from SW.



Site 03707

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03707 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	261630	322922
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	<p>An arched masonry bridge carries the embanked railway over a road. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins; they are 9.15m deep. The arch is of segmental profile and spans 7.72m. Its soffit is of squared random rubble and its voussoirs are rusticated, with a margined edge to the arch. The spandrels are also of squared random rubble, with a dressed stone coping. Wing walls, also of squared rubble, project at right angles from both sides.</p> <p>A short distance west of the bridge is a skew semicircular-arched stream culvert.</p>					
Interest	Architectural; Historical; Setting					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is also a prominent feature along the road and is of regional industrial heritage significance, meriting inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03707_01 FWH 11/10/2007 North elevation.



Site 03708

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03708 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	261401	322886
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched masonry bridge carries the embanked railway over an accommodation road. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins; they are 8.64m deep. The arch is of semicircular profile and spans 3.72m. Its soffit is of squared random rubble and its voussoirs are rusticated, with a margined edge to the arch. The spandrels are also of squared random rubble, with a rusticated and margined stone coping. Wing walls, also of squared rubble, project at right angles from both sides; they are coped with rusticated and margined blocks. The end of the NE wing wall is missing. The embankment has been removed eastwards of the bridge.

Interest Architectural; Historical

Evaluation This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03708_01 FWH 11/10/2007 North elevation.



Site 03709

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03709 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	261073	322838
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03710

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03710 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO018	Grid	260946	322818
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03711

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03711 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO018	Grid	260807	322798
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	
Description	An arched masonry bridge carries the embanked railway over a road. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins; they are 9.15m deep. The arch is of segmental profile and spans 7.60m. Its soffit is of squared random rubble and its voussoirs are rusticated, with a margined edge to the arch. The spandrels are also of squared random rubble, with a dressed stone coping. Wing walls, also of squared rubble, project at right angles from both sides. They are coped with dressed stone blocks and terminate in rusticated and margined piers.					
Interest	Architectural; Historical; Setting					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is also a prominent feature along the road and is of regional industrial heritage significance, meriting inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03711_01 FWH 11/10/2007 South elevation.



Site 03712

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Deck replaced in 1954 by Great Northern Railway Board. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03712 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	260555	322765
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description A beam bridge carries an accommodation track over the former line, here in a cutting. The original deck has been replaced with concrete. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins. The original clear span between the abutments was 9.12m. There are tapered random rubble buttresses to each side; they also have rusticated and margined quoins. The replacement deck comprises two pre-cast reinforced-concrete slabs, supported towards their north end by a concrete pier. The south span measures 5.05m and the north one 3.36m. Along both edges of the deck is inscribed "GNR 1954". The parapets comprise post and wire fences 4.07m apart. The approach walls are of random rubble, with rusticated/margined copings which set up from south to north. There is a similarly detailed string course along their base.

Interest Architectural; Historical; Technical

Evaluation This bridge is primarily of historical interest due to the attested date of its replacement reinforced-concrete deck, reflecting its upgrading by the Great Northern Railway Board (created in 1953). It is of local industrial heritage significance.

Significance Local

03712_01 FWH 11/10/2007 East elevation.



03712_02 FWH 11/10/2007 GNR inscription along east edge of deck.



Site 03713

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03713 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO017	Grid	260289 322755
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Poor	Use	Disused
Description	A wrought-iron sunburst and the roughly-dressed stone pillar from which it is hung survive on the north side of the line. The bottom half of another gatepost survives on the south side of the line.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03713_01 FWH 11/10/2007 North gate and post, from south.



Site**03714****Drumate Crossing; Drumate Halt**

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03714 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	259863	322803
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03714 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO017	Grid	259858	322813
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	A three-bay, single-storey former level crossing keeper's house aligned east-west on the SW side of the crossing. Gabled natural slate roof, with plain bargeboards, boxed eaves and cement-rendered chimney. Walls are also rendered with cement. All openings have square heads. The south elevation (facing the track) has a small porch to the middle bay, with a tongue-and-groove door. There is a window opening to the west and middle bay. The west gable is blank. The north elevation has a window to the middle bay. The east gable (facing the road) has a window. The windows are of assorted design - 2/2, 3/3 and 3/6 and all have concrete cills save that on the north elevation which is cill-less.					
Interest	Architectural; Historical					
Evaluation	This house retains much of its original character and has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

Component	03714 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO017	Grid	259869 322802
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of the hardstand associated with this request halt survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03714_01 FWH 11/10/2007 Level crossing house from SE.



Site 03715

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03715 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	259532	322890
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	A beam bridge formerly carried an accommodation track over the former line, here in a cutting. The deck has been lifted and the abutments completely buried in order to improve access along the track to agricultural buildings on the north side of the track. The three principal girders have been discarded to the NE end of the former bridge. They are of wrought-iron lattice type and measure 48cm x 24cm in cross-section (their ends were buried so their length could not be determined). According to the owner, the deck was timber. The cutting has been partly removed to the east and completely infilled to the west.					
Interest	None					
Evaluation	No special industrial heritage significance. Measured drawings of the remains of the lattice girders should be made.					
Significance	Record only			Action	Measured drawings	

03715_01 FWH 11/10/2007 Discarded lattice girder bridge beams.



Site 03716

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03716 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	259150	323055
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03717

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Deck replaced in 1953 by Great Northern Railway Board. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03717 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	258997	323103
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description A beam bridge carries an accommodation track over the former line, here in a cutting. The original deck has been replaced with concrete. Both the original masonry abutments have been faced with concrete. The replacement deck comprises a reinforced-concrete slab, cast in situ and spanning 8.45m. Along both edges of the deck is inscribed "GNR 1953". The parapets comprise five-bar tubular steel railings on angle-iron posts; they are 3.87m apart. The approach walls are of random rubble with a concrete coping; there are also vestiges of a chamfered string course along their base. The SE section of the wall has been replaced with concrete blockwork. The cutting is partly infilled under and to the east of the bridge.

Interest Architectural; Historical; Technical

Evaluation This bridge is primarily of historical interest due to the attested date of its replacement reinforced-concrete deck, reflecting its upgrading by the Great Northern Railway Board (created in 1953). It is of local industrial heritage significance.

Significance Local

03717_01 FWH 11/10/2007 West elevation.



03717_02 FWH 11/10/2007 Inscription cut into east edge of concrete deck.



Site 03718

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03718 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	258708	323150
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03719

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03719 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO017	Grid	258616 323161
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Poor	Use	Disused
Description	A wrought-iron sunburst gate and two stone posts survive on the north side of the accommodation crossing. There is a small recess on the track face of the north-easts post, probably for a 'no trespassing' sign. Nothing survives on the south side.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03719_01 FWH 11/10/2007 North gate from south.



Site 03720

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03720 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	258377	323186
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03721

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03721 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	258198	323213
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate has been re-hung from a (later) concrete post across the west side of this accommodation crossing.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Site 03722

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03722 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	258056	323242
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This cutting has been infilled and a new lane built across it. There are no traces of the former accommodation road bridge.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03723

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03723 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	257949	323282
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this accommodation crossing survives. The land has been reclaimed for agriculture.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03724

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03724 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	257780	323357
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	Two wrought-iron sunburst gates and their two posts survive on both sides of this accommodation crossing. The south gate has been struck by a vehicle and is severely bent. Parts of the north gate are also missing. Each gate measures 3.05m long by 1.45m high (10ft x 4ft 9in). The posts are 30cm square in cross section and 3.08m apart.					
Interest	Architectural; Historical					
Evaluation	The completeness of these gates and association with the railway make them of local industrial heritage significance.					
Significance	Local					

03724_01 FWH 11/10/2007 North gate from north. Note south gate behind.



Site **03725** **Drumshannon Crossing; Drumshannon Halt**

History Line opened by the Dundalk & Enniskillen Railway in August 1855. According to Mills (1903), the level crossing keeper's house dates from the period 1877-1903. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03725 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	257592	323458
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this road crossing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03725 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO017	Grid	257591	323472
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A refurbished three-bay, single-storey former level crossing house aligned NW-SE on the SW side of the road crossing. Gabled natural slate roof, with plain bargeboards, boxed eaves and red brick chimney. Walls are painted and rendered with cement. All openings have segmental heads. The SW elevation (facing the track) has a gabled porch to the middle bay with a door to the right cheek and window to its gable. This porch is probably a later addition. There is also windows to each of the three bays. The SW gable was not inspected. There are two windows to the NE elevation. The SE gable is blank. All doors and windows are uPVC replacements.					
Interest	None					
Evaluation	The original character of this house has been diminished by its refurbishment and it is no longer of special industrial heritage significance.					
Significance	Record only					

Component	03725 03	Type	Hardstand
County	Monaghan	OS 6-inch map	MO017 Grid 257597 323455
Surveyed?	Y	Survey date	11/10/2007 Surveyor Fred Hamond
Completeness	No visible remain	Condition	N/A Use N/A
Description	No traces of the hardstand associated with this request halt survive.		
Interest	None		
Evaluation	No industrial heritage significance.		
Significance	Record only		

03725_01 FWH 11/10/2007 South-west elevation of level crossing house.



Site 03726

History Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03726 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	256844	324014
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Road bridge	
Description	An arched bridge carries a road over the former railway, here in a cutting. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins. A chamfered string course runs through the arch at spring level and around the quoins. Buttresses rise at each end to parapet level; they have rusticated and margined quoins. The arch is of semi-elliptical profile and spans 9.17m. The soffit is of squared random rubble and the voussoirs of rusticated masonry blocks with a margined edge to the arch. The spandrels and parapets are of squared random rubble. A rusticated and margined string course runs along the base of the parapets. The latter are coped with rusticated and margined stone blocks and terminate in out-projecting similarly-detailed piers; they are 7.67m apart. The SW end of the parapets and pier are missing. The road approach from the south is ramped.					
Interest	Architectural; Historical					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03726_01 FWH 11/10/2007 East elevation. Note Newbliss Station buildings behind.



03726_02 FWH 25/06/2007 Road bridge, from west.



03726_03 FWH 11/10/2007 West elevation.



History

Line opened by the Dundalk & Enniskillen Railway in August 1855. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

The signal box (component 5) probably dates from the period 1877-1903 and is the work of W.H. Mills, the GNR(I)'s Chief Engineer at that time. The store (6) is probably of mid 20th century date.

03801_01 Drew Donaldson 06/08/1955 Source C. Friel Collection: DDMW8.
Newbliss Station from the east.



03801_02 R.M. Arnold 23/02/1957 Source C. Friel Collection.
Newbliss Station with a Dundalk to Clones train.



03801_03 Unknown Source Johnson 2006, p. 27.
Newbliss Station. Note the lamp between the lines to illuminate the crossing.

Component	03801 01	Type	Station building; Station house			
County	Monaghan	OS 6-inch map	MO017	Grid	256703	324039
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	Detached multiple-bay former station building, built c.1855, comprising two-storey gabled station master's house to east and single-storey gabled station building (ticket office, waiting rooms, toilets etc) to west. The two sections are connected by single-storey range with sandstone arcade to front and rear elevations. The house had a					

separate entrance on east elevation.

Replacement pitched artificial slate roof with replacement rainwater goods supported on carved sandstone corbel course. Roof now extends over single-storey central range where previously there was a pitched roof on the main roof and slate lean-to roofs over arcaded porches on front and rear elevations. Rendered walls with ruled and lined detail with sandstone chamfered plinth course. Coursed rubble stone wall to basement area of station house. Sandstone quoins visible beneath sections of removed render on platform elevation at north. Chamfered sandstone piers to Tudor-arched arcade to front and rear elevations. Carved sandstone quatrefoil to west gable of single-storey section. Square-headed window openings with chamfered sandstone surrounds and replacement timber casement windows. Large window openings on gabled elevations have sandstone labels. Sandstone canted-bay window on north elevation of single-storey section. Tudor-arched door openings set within arcaded porches on front and rear elevations. Chamfered sandstone door surround and replacement timber double doors. Sandstone steps access entrance on south elevation. Door on north elevation accesses down platform.

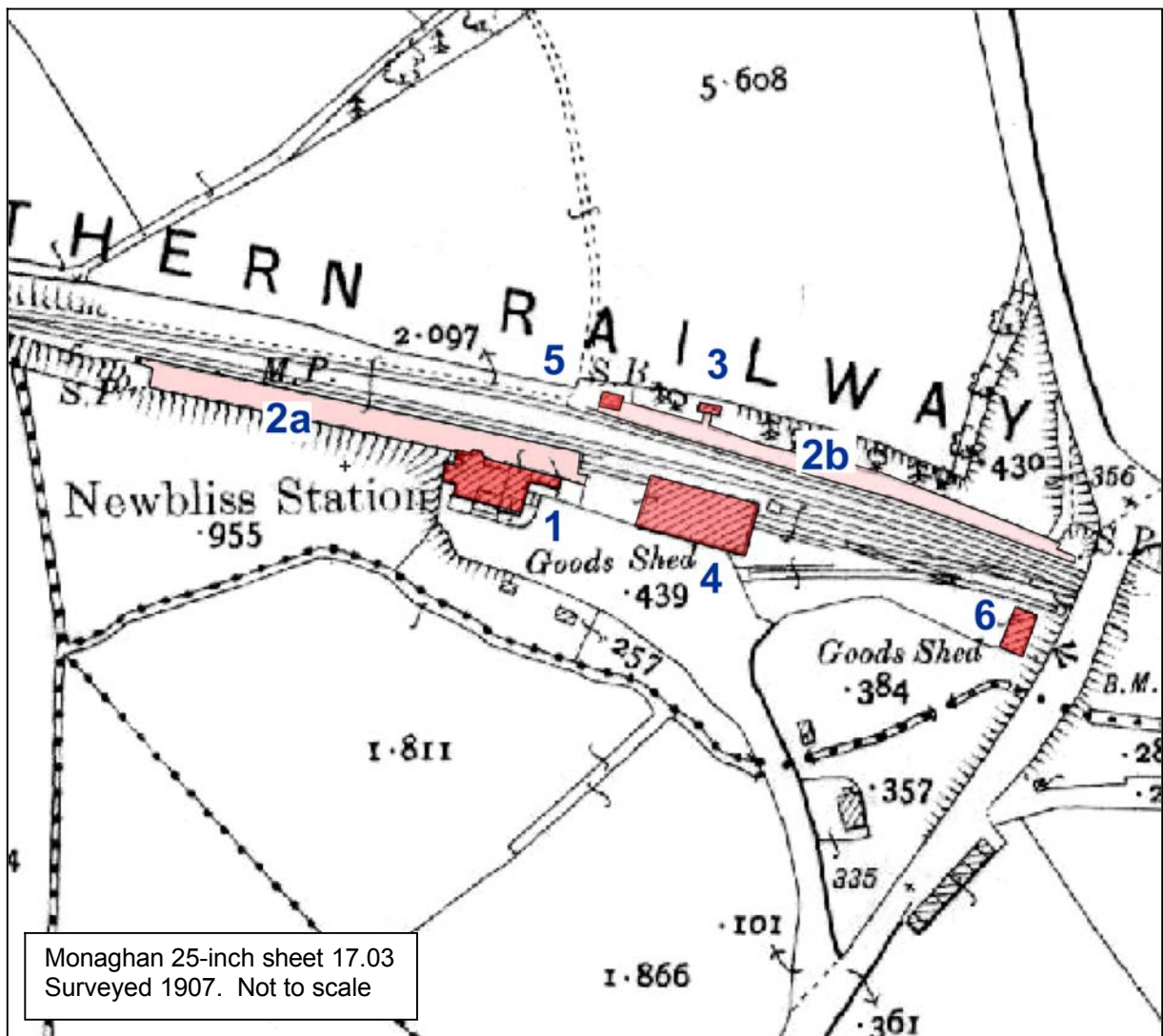
No original interior features or fittings survive. The building was in a derelict condition until present owners began works.

Interest Architectural; Historical; Social; Group; Setting

Evaluation Although undergoing restoration, the essential architectural character of this building has been largely retained. It is the most prominent feature of this relatively intact station complex and was of social significance when the station was operational. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Significance Regional Action RPS

Component	03801	02a	Type	Platform		
County	Monaghan		OS 6-inch map	MO017	Grid	256666 324054
Surveyed?	Y		Survey date	31/10/2007	Surveyor	AR&R
Completeness	Complete		Condition	Fair	Use	Disused
Description	This was the 'down' platform along north side of station building. Rubble limestone platform wall with stone kerbstones to edge. Layer of concrete covers surface of stone kerbstone. Some exposed stone kerbstone to east end.					
Interest	Historical; Group					
Evaluation	Primarily of interest because of its functional association with the station building and group value with the complex as a whole. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional				Action	RPS



Monaghan 25-inch sheet 17.03
 Surveyed 1907. Not to scale

Component	03801 02b	Type	Platform				
County	Monaghan	OS 6-inch map	MO017	Grid	256771	324043	
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R		
Completeness	Complete	Condition	Fair	Use	Disused		
Description	This was the 'up' platform. Unlike other stations, it was not connected to the station building by a footbridge over the line, but had a separate entrance off the road (across the road bridge). Different materials suggest that it was constructed in two phases A rubble stone platform wall with sandstone kerbstones to edge runs SE from signal box end for approximately half the length of the platform. It then continues towards bridge as a rubble stone wall with concrete kerbstone; concave moulding to edge of kerbstone.						
Interest	Historical; Group						
Evaluation	Primarily of interest because of its group value within the station complex. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.						
Significance	Regional			Action	RPS		

Component	03801 03	Type	Waiting shelter				
County	Monaghan	OS 6-inch map	MO017	Grid	256753	324057	
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R		
Completeness	Complete	Condition	Poor	Use	Disused		
Description	Detached rectangular-plan single-storey former waiting shelter on up platform. Hipped roof with terracotta pantiles to rear and side pitches and a replacement fibre cement tile to front; also terracotta ridge tiles. Snecked stone walls. Large continuous window opening to front elevation, broken only by the central door opening, wraps around corners to side elevations to give an uninterrupted view of the platform from inside waiting room. Timber paned window with concrete sill. Windows are fixed. Square-headed door opening to centre of front elevation with timber and glazed door. Bottom section of door missing. Interior comprises rendered walls and timber underside of roof structure.						
Interest	Architectural; Historical; Group						
Evaluation	Primarily of interest because of its functional association with the station building and group value with the complex as a whole. The pantiles are the only such roof of any building on the Louth-Monaghan section of the line. The building is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.						
Significance	Regional			Action	RPS		

Component	03801 04	Type	Goods shed			
County	Monaghan	OS 6-inch map	MO017	Grid	256750	324035
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	Former goods shed abutted to all elevations by later additions (principally a jam factory built by the owner's father). Roof no longer survives. Rubble stone walls to goods shed with sandstone quoins. Redbrick round-headed arches located at north end of both gables for locos. The arch on west gable is infilled and a later square-headed window opening inserted. The arch on east gable has replacement timber battened double doors.					
Interest	Historical; Group					
Evaluation	Although ruinous, this was an important part of the complex when operational. Apart from Clones, it is the only surviving goods shed on the line within Louth and Monaghan. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

Component	03801 05	Type	Signal box			
County	Monaghan	OS 6-inch map	MO017	Grid	256730	324060
Surveyed?	Y	Survey date	31/10/2007	Surveyor	AR&R	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	<p>Located at west end of up platform, opposite the station building. Comprises a brick base with timber and glazed cabin above. Pitched slate roof to structure with terracotta ridge tiles. Decorative brackets to eaves. Base has redbrick walls laid in English bond with rectangular recessed niches to rail-facing south elevation. Rear elevation not accessible.</p> <p>Glazed timber cabin above base with timber weatherboarding to east gable. Large glazed panels to centre of south elevation flanked by horizontal sliding windows. Square-headed door opening on east gable with glazed timber door. Timber stairs accessing this door are missing.</p> <p>Internally, the floor was replaced when the building was converted to an office.</p>					
Interest	Architectural; Historical; Group					
Evaluation	Although its equipment has been removed, this signal box is largely intact and an integral part of the station complex. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

Component	03801 06	Type	Store		
County	Monaghan	OS 6-inch map	MO017	Grid	256826 324005
Surveyed?	Y	Survey date	25/06/2007	Surveyor	Fred Hamond
Completeness	Traces	Condition	Poor	Use	Disused
Description	This was a small concrete and timber walled building with concrete floor, little of which now remains. It was probably a store. Immediately NE is a buffer stop.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

03801_01 FWH 25/06/2007 General view of site from east (road bridge)



03801_02 FWH 25/06/2007 Station building, from SE.



03801_03 FWH 25/06/2007 Station building and down platform, from east.



03801_04 AR&R 31/10/2007 North (rear) elevation of station.



03801_05 AR&R 31/10/2007 South-facing single-storey gable to front elevation of station.



03801_06 AR&R 31/10/2007 Detail of steps and arcade to front elevation of station.



03801_07 AR&R 31/10/2007 Arcade to front elevation of station.



03801_08 AR&R 31/10/2007 Sandstone arches to front elevation of station.



03801_09 AR&R 31/10/2007 Cantled-bay window to rear elevation of station.



03801_10 AR&R 31/10/2007 Window opening within front elevation porch of station.



03801_11 AR&R 31/10/2007 Window opening on north-facing two-storey gabled elevation of station.



03801_12 AR&R 31/10/2007 Door opening accessing former two-storey station house



03801_13 AR&R 31/10/2007 Detail of tooled sandstone door reveal in station.



03801_14 AR&R 31/10/2007 Quatrefoil to single-storey gabled front elevation of station.



03801_15 AR&R 31/10/2007 Down platform.



03801_16 AR&R 31/10/2007 Concrete surface to sandstone kerbstones on down platform



03801_17 AR&R 31/10/2007 Exposed sandstone kerbstones at east end of down platform



03801_18 AR&R 31/10/2007 Detail of sandstone kerbstones with concrete surface to down platform.



03801_19 FWH 25/06/2007 Up platform, from SE.



03801_20 AR&R 31/10/2007 Up platform to south of waiting room.



03801_21 AR&R 31/10/2007 Detail of up platform wall with sandstone.



03801_22 AR&R 31/10/2007 Detail of up platform wall with concrete.



03801_23 AR&R 31/10/2007 Detail of join in up platform where sandstone and concrete kerbstones meet.



03801_24 FWH 25/06/2007 Waiting room, from SW.



03801_25 AR&R 31/10/2007 Waiting room from SE.



03801_26 AR&R 31/10/2007 Door opening in waiting room.



03801_27 AR&R 31/10/2007 Detail of window n waiting room.



03801_28 AR&R 31/10/2007 Detail of terracotta tiles to roof of waiting room.



03801_29 AR&R 31/10/2007 Detail of concrete sill to window of waiting room.



03801_30 AR&R 31/10/2007 View of interior of waiting room.



03801_31 AR&R 31/10/2007 Replacement tiles to front pitch of waiting room.



03801_32 FWH 25/06/2007 Goods shed, from west.



03801_33 AR&R 31/10/2007 General view of goods shed, platform and signal box, from east.



03801_34 AR&R 31/10/2007 West gable end wall of goods shed.



03801_35 AR&R 31/10/2007 Arched opening on west gable of goods shed.



03801_36 AR&R 31/10/2007 SW end of goods shed with modern sheds abutting



03801_37 AR&R 31/10/2007 Arched opening on east gable of goods shed, from inside.



03801_38 FWH 25/06/2007 Signal box, from SE.



03801_39 AR&R 31/10/2007 Signal box from SW.



03801_40 AR&R 31/10/2007 East gable of signal box.



03801_41 AR&R 31/10/2007 Lettering on east gable of signal box.



03801_42 AR&R 31/10/2007 Timber brackets to eaves of signal box.



03801_43 AR&R 31/10/2007 Window and ceiling to interior of signal box.



03801_44 AR&R 31/10/2007 Door opening to signal box.



03801_45 AR&R 31/10/2007 King-post truss to interior of signal box.



03801_46 AR&R 31/10/2007 Door to east gable of brick base of signal box.



Site 03802

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03802 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO017	Grid	256487 324095
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Fair	Use	Disused
Description	A wrought-iron sunburst gate and two stone gateposts survive on the north side of this accommodation crossing. Only the two stone posts survive on the south side.				
Interest	Architectural; Historical				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03802_01 FWH 11/10/2007 North gate and posts, from north.



Site 03803

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03803 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	256247	324136
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Road bridge	
Description	<p>A skew arched bridge carries the embanked railway line over a road and footpath. The abutments are of squared random rubble, embellished with rusticated and margined quoins. The east abutment extends north beyond the end of the arch to incorporate a wing wall and is 9.83m deep; the west abutment is only 6.22m deep. A rusticated and margined string course runs through the arch at spring level and around the quoins. Tapered squared random rubble buttresses rise at each end, except at the SW end where the face of the bridge is battered; they have rusticated and margined quoins. The arch is of segmental profile and spans 7.65m, measured orthogonally; it is 7.89m on the skew. The soffit is of squared random rubble, laid orthogonally, and the voussoirs of rusticated masonry blocks with a margined edge to the arch. The spandrels and parapets are of squared random rubble. A rusticated and margined string course runs along the base of the parapets. The latter are coped with rusticated and margined stone blocks. The SW end of the parapets and pier are missing.</p> <p>On the west side of the road is a smaller semicircular arch which conveys a stream under the embankment. It is detailed as the main bridge, and spans 3.98m measured orthogonally, and 3.13m on the skew.</p> <p>An accommodation track now runs along the former line.</p>					
Interest	Architectural; Historical; Setting					
Evaluation	This masonry arch bridge is a good example of its type and has an historical association with the railway. It also incorporates a second arch for a stream and is a prominent feature of the road. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03803_01 FWH 11/10/2007 North elevation; downstream end of stream arch at right.



03803_02 FWH 11/10/2007 South elevation; upstream end of stream arch at left.



Site 03804

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03804 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO017	Grid	255786	324229
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	This masonry arch bridge carries the embanked railway over a stream. The abutments are of random rubble with plain quoins; they are 10.83m deep. The arch is of semicircular profile and spans 3.05m. Its voussoirs are of rusticated blocks and there is a margined edge to the arch. The spandrels are also of random rubble and are coped with concrete. There are similarly detailed angled wing walls at each end. The abutments are now footed in concrete. Part of the SW wing wall has been undercut , causing it to subside.					
Interest	Architectural; Historical					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

03804_01 FWH 11/10/2007 Upstream (N) elevation.



Site 03805

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03805 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	255613	324301
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03806

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03806 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO017	Grid	255460	324379
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	This skew masonry arch bridge carries the embanked railway over a stream. The abutments are of split random rubble with plain quoins; they are 7.87m deep. The arch is of semicircular profile and spans 3.02m, measured orthogonally and 3.26m on the skew. Its voussoirs are of rusticated blocks and there is a margined edge to the arch. The spandrels are also of random rubble and are coped with concrete blocks. There are similarly detailed angled wing walls at each end.					
Interest	Architectural; Historical					
Evaluation	This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.					
Significance	Local					

03806_01 FWH 11/10/2007 Upstream (N) elevation.



Site 03807

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03807 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO017	Grid	255275	324480
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Poor	Use	Disused	
Description	A wrought-iron sunburst gate and two stone gateposts survive on the north side of this accommodation crossing. An identical gate and one post survive on the south side. The posts have rusticated and margined edges.					
Interest	Architectural; Historical					
Evaluation	The relatively completeness of this crossing and its historical association with the railway make it of local industrial heritage significance.					
Significance	Local					

03807_01 FWH 11/10/2007 South gate and posts, from south. Note opposite gate in background.



Site 03808

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03808 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	255095	324560
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description A tall overgrown arched bridge carries the embanked railway line over a road. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins. The abutments extend beyond the arch on the north side to incorporate the embankment wing walls; the arch soffit is 4.92m deep, whereas the overall abutment depth is 8.68m. A chamfered string course runs through the arch at spring level and around the quoins. It continues as copings down the north abutments. Vegetation now obscures the arch, but it appears to be of segmental profile. It spans 7.53m and is 7.15m high to the soffit's crown. The soffit is of squared random rubble, and the voussoirs of rusticated masonry blocks with a margined edge to the arch. The spandrels are overgrown, but are probably of squared random rubble with copings. There are angled random rubble wing walls to each end, also with rusticated copings. The embankment has been removed on the west side of the bridge, exposing several random rubble pilasters. The road approaches are dog-legged.

Interest Architectural; Historical; Setting

Evaluation This bridge is a good example of its type and has an historical association with the railway. Its height makes it a particularly prominent feature of the road. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Significance Regional Action RPS

03808_01 FWH 11/10/2007 North elevation.



03808_02 FWH 11/10/2007 South elevation. Note missing embankment at left.



Site 03809

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03809 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	254667	324673
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description An arched bridge carries a road over the former railway, here in a cutting. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins. A chamfered string course runs through the arch at spring level and around the quoins. There are tapered random rubble buttresses at each end; they have rusticated and margined quoins. The arch is of semi-elliptical profile and spans 9.21m. Its soffit is of squared random rubble and the voussoirs of rusticated masonry blocks with a margined edge to the arch. The spandrels and parapets are of random rubble, without a string course between. The latter are coped with roughly-dressed masonry blocks and there is brickwork to their ends; they are 3.65m apart. The cutting is now partly infilled immediately west of the bridge.

Interest Architectural; Historical

Evaluation This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03809_01 FWH 11/10/2007 West elevation.



Site 03810

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03810 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	254472	324686
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Road bridge	

Description An arched bridge carries a road over the former railway, here in a cutting. The abutments are of squared random rubble, brought to courses and embellished with rusticated and margined quoins. There are shallow tapered buttresses to each end, with similarly-detailed quoins. A chamfered string course runs through the arch at spring level and around the quoins. There are tapered random rubble buttresses at each end; they have rusticated and margined quoins. The arch is of semi-elliptical profile and spans 9.10m. Its soffit is of rock-faced squared random rubble and the voussoirs of rusticated masonry blocks with a margined edge to the arch. The spandrels and parapets are of random rubble, brought to courses and without a string course between. The latter are coped with roughly-dressed masonry blocks. They are 3.73m apart and terminate in out-projecting rusticated/margined piers. The cutting is infilled at the west end of the bridge.

Interest Architectural; Historical

Evaluation This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03810_01 FWH 11/10/2007 West elevation.



Site 03811

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03811 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	253932	324754
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	A skew arched bridge carries the embanked railway line over a road. The abutments are of rock-faced and randomly-sized masonry blocks, laid randomly and embellished with rusticated and margined quoins; they are 5.74m deep. A chamfered string course runs through the arch at spring level and around the quoins. There are tapered buttresses at each end, of regularly-laid rock-faced masonry blocks and with rusticated and margined quoins. The arch is of semi-elliptical profile and spans 7.63m, measured orthogonally/ 8.07m on the skew. It has a very low rise relative to its span. Its soffit is of masonry blocks, laid orthogonally. The voussoirs are of rusticated masonry blocks, stepped into the spandrels, and with a margined edge to the arch. The spandrels are of squared random rubble with dressed stone copings and a course of masonry blocks over. There are curved squared random rubble wing walls to each end, with chamfered copings; they terminate in low rusticated and margined piers.					
Interest	Architectural; Historical; Setting					
Evaluation	This bridge has an unusually flat semi-elliptical arch and also has an historical association with the railway. It is also a prominent feature along the road and is of regional industrial heritage significance, meriting inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

03811_01 FWH 11/10/2007 North elevation.



03811_02 FWH 11/10/2007 South elevation.



Site 03812

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03812 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	253855	324779
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description An arched bridge carries the embanked railway line over an accommodation track. The abutments are of roughly-dressed, randomly-sized masonry blocks, laid randomly and embellished with rusticated and margined quoins; they are 4.65m deep. A chamfered string course runs through the arch at spring level and around the quoins. There are shallow tapered buttresses at each end, of squared random rubble. The arch is of semicircular profile and spans 3.03m. Its soffit is of random rubble and the voussoirs of rusticated masonry blocks, with a margined edge to the arch. The spandrels and parapet are of squared random rubble. A string course runs along the base of the parapets. The latter are coped and terminate in out-projecting piers. The buttresses continue as curved coped wing walls which terminate in rusticated/margined piers.

Interest Architectural; Historical

Evaluation This bridge is a good example of its type and has an historical association with the railway. It is of local industrial heritage significance.

Significance Local

03812_01 FWH 11/10/2007 North elevation.



History

Line opened by the Dundalk & Enniskillen Railway in July 1858. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Old photographs show the original bridge (component 1) to have been a multi-span lattice girder bridge, each span with a segmental profile. The spans were supported on pairs of cross-braced metal uprights. The abutments were of stone and had curving wing walls. The bridge was replaced in 1925 (component 2). Old photographs show the replacement to have had steel plate girder spans and four-bar parapet railings.

03813_01 Unknown

Source Murray 1944, plate 44.

Original lattice girder viaduct.



03813_02 Unknown

Source Murray 1944, plate 45.

Replacement plate girder viaduct.



Component	03813 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO017	Grid	253771	324799
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Fair	Use	Disused	
Description	No traces of this bridge survive apart from its masonry abutments and curved wing walls, all of which are incorporated in its replacement (component 2).					
Interest	Historical; Technical; Rarity					
Evaluation	No special industrial heritage significance.					

Significance Record only

Component	03813 02	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO017	Grid	253771	324799
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Substantial remai	Condition	Fair	Use	Disused	
Description	<p>An eight-span beam bridge, c.115m long, formerly carried the railway over an ornamental lake in the grounds of Ballynure House. The stone abutments of the original bridge (component 1) survive, but the piers and deck date from its replacement. The deck has been lifted since the line's closure.</p> <p>The east abutment is of rock-faced masonry blocks, laid to courses and with a concrete coping; some bricks are incorporated into its upper section. There are tapered reinforced-concrete buttresses to each side of the abutment; they are embellished with vee banding. Behind the abutments are the original curved wing walls, of rock-faced random blocks and with dressed stone copings. The face of the original stone abutment at the west end is now obscured by the later concrete one which also has vee banding. The original curved stone wing walls also survive.</p> <p>The abutments are c.110m apart and between them are seven concrete piers. The ends of the piers taper inwards towards their tops and step out at the bottom. The piers and west abutment have concrete pillow blocks which formerly supported the girder spans; they are 6.48m long by 1.98m deep and are 46cm thick. The spans average 11.6m (38ft). The lake is now drained and a narrow stream runs between the third and fourth piers from the east end. An accommodation track runs between the fifth and sixth piers.</p> <p>This area is now very overgrown, hindering a full appreciation of the scale of this viaduct.</p>					
Interest	Historical; Technical; Rarity					
Evaluation	Although only partially surviving this viaduct is of interest because of its scale and replacement of the original piers with concrete ones in the 1900s. It is the only bridge with these features on the Louth/Monaghan section of this line. The remains are of regional industrial heritage significance and merits inclusion in the Record of Monuments & Places					
Significance	Regional			Action	RMP	

03813_01 FWH 11/10/2007 General view of west end of viaduct, from west.



03813_02 FWH 11/10/2007 East abutment, from SE.



03813_03 FWH 11/10/2007 West abutment.



03813_04 FWH 11/10/2007 Seventh and sixth piers from east end, from west.



Site 03814

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03814 01	Type	Bridge (road/rail)			
County	Monaghan	OS 6-inch map	MO017	Grid	253494	324807
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	

Description A triple-arch masonry bridge carries an accommodation track over the former railway, here in an exceptionally deep cutting in the grounds of Ballynure House. The abutments and piers are of regularly-laid rock-faced masonry blocks, which are embellished with finely-dressed margined quoins. A chamfered string course runs through the arches at spring level and around the quoins. The edges of the piers taper inwards as they rise; they are 4.90m deep at their base. All the arches have shallow segmental profiles; the line ran through the middle arch. Their soffits are of rock-faced blocks and the voussoirs are of finely-dressed, vee-jointed blocks. The faces of the piers also taper inwards as they rise. Accordingly, the north and south arches span 4.52m and 4.50m respectively just below their spring courses; the middle arch spans 4.30m at the base of its piers. The spandrels are of squared random rubble, with a dressed coping on top of which are wrought-iron lattice railings with a timber handrail across their tops. Much of the railing is now missing.

Interest Architectural; Historical; Rarity

Evaluation This is an exceptionally well detailed accommodation bridge over the railway and somewhat unusual in being a triple-arch span rather than a beam bridge. The middle arch is also extremely narrow, at only 4.3m across. Although only an accommodation bridge, it is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.

Significance Regional Action RPS

03814_01 FWH 11/10/2007 East elevation.



03814_02 FWH 11/10/2007 West elevation of middle and south arch.



03814_03 FWH 11/10/2007 Parapet detail at NE end.



History Tunnel dates from the construction of this stretch of line in the 1850s. It collapsed with the death of upwards of 11 men and was abandoned in favour of a deep cutting to its north.

Component	03815 01	Type	Tunnel		
County	Monaghan	OS 6-inch map	MO017	Grid	253589 324740
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Poor	Use	Disused

Description Remains of now-buried tunnel on both sides of steep hill. At the east end is a depression at the west end of which is a random rubble wall with a opening into a small subterranean chamber. Its sides are of corbelled stone slabs, with lintels at the top. It measures 4.6m long by 0.80m wide and is 1m high. It was probably much larger but has been reduced to its present size due to the accumulation of silt. A short distance downhill to the east is a tree-covered spoil heap of excavated earth and rubble stones.
 Some 230m away, at bottom of the other side of the hill, a cut into the hillside marks the west end of the tunnel. Unfortunately fly tipping, overgrowth and silting precluded any access to the actual entrance to the tunnel.
 Just east of the hill's summit is a 1.2m deep depression which probably marks where the tunnel collapsed.

Interest Historical; Rarity

Evaluation Although never completed, this tunnel is of historical interest in showing the line's intended route. It is also a monument to the men who were killed when it collapsed during construction. It is of regional industrial heritage interest. Although it is in the Record of Protected Structures (Monaghan Local 59), inclusion in the Record of Monuments & Places may be a more appropriate protective measure.

Significance Regional Protection Monaghan Action RMP
 RPS Local 59

03815_01 FWH 23/10/2007 View of drumlin from east. Tree clump marks spoil heap from east end of tunnel.



03815_02 FWH 23/10/2007 East end of tunnel, from west, with spoil heap below.



03815_03 FWH 23/10/2007 Face of east end of tunnel.



03815_04 FWH 23/10/2007 Looking inside east end of tunnel



03815_05 FWH 23/10/2007 Depression near summit of hill (marking collapse).



03815_06 FWH 23/10/2007 Overgrown west end of tunnel, from west.



Site 03816

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03816 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO017	Grid	253186	324822
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Disused	
Description	<p>A soaring arched bridge carries the embanked railway line over the River Finn; an accommodation track also runs under its east (left bank), such that only the west abutment is visible. It is of rock-faced margined blocks, laid to courses and embellished with rusticated and margined quoins. The abutments extend beyond the north end of the arch to incorporate the embankment wing walls. The depth of the arch soffit is 5.03m, whereas the abutments are 8.53m deep. A chamfered string course runs through the arch at spring level and around the quoins. The arch is of semicircular profile and spans 19.67m (64ft 6in). Its soffit is of rock-faced stone blocks and the voussoirs of rusticated masonry blocks, with a margined edge to the arch. The spandrels are of roughly-dressed stone blocks, laid to regular courses and coped with chamfered stone blocks set 3.95m apart. The line is c.14.7m above the river. Wing walls project at right angles. They are of rock-faced, randomly-sized masonry blocks, laid randomly and terminating in rusticated and margined piers. A 3.46m wide track runs along the face of the east abutment. It has been raised level with the string course at arch spring level. Its retaining wall to the river is detailed as the west abutment.</p> <p>The railway embankment survives at the east end of the bridge but has been completely removed at the west end to reveal a rubble stone buttress.</p>					
Interest	Architectural; Historical; Setting					
Evaluation	This is the largest single-span masonry bridge on the Louth/Monaghan section of the Irish North-West (and probably on the entire line, if not in Ireland). It is constructed to an extremely high standard and a prominent landscape feature. It is of national industrial heritage significance and merits its current inclusion in the Record of Protected Structures (Monaghan Local 58).					
Significance	National	Protection	Monaghan RPS Local 58			

03816_01 FWH 11/10/2007 North elevation, from NE.



03816_02 FWH 11/10/2007 South elevation, from SE.



03816_03 FWH 23/10/2007 Bridge from SW.



03816_04 FWH 11/10/2007 West abutment and arch soffit.



03816_05

FWH

11/10/2007

Terminal pier on NE wing wall.

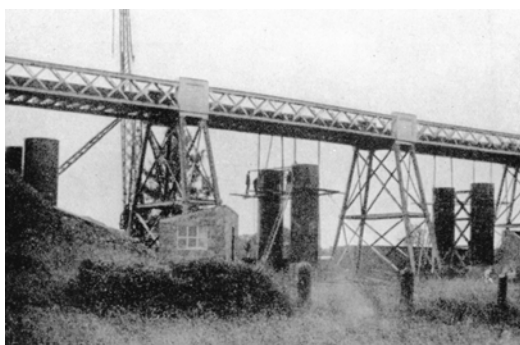


Site 03817

History Line opened by the Dundalk & Enniskillen Railway in July 1858. The original timber deck was seemingly replaced with lattice girder spans (component 1). A photograph shows these spans to be supported on braced metal trestles. This entire structure was then replaced with steel plate girder spans on cylindrical piers c.1925 (component 2). Another photograph shows a four-bar parapet railings. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

03817_01 Unknown Source Murray 1944, plate 46.

Lattice girder viaduct. Although the location of this photograph is not stated, the viaduct's number of spans and topographical context suggest that it is probably at this site.



03817_02 Unknown Source Murray 1944, plate 47.

Replacement plate girder viaduct.



Component	03817 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	253006	324858
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of the original bridge survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03817 02	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	253006	324858
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	A seven-span metal beam bridge, c.105m long, formerly carried the line over a valley and accommodation track. The embankments at each end and the deck have been lifted, leaving only the supporting columns. These comprise pairs of riveted metal cylinders, 1.83m in diameter and filled with concrete; the columns in the middle of the structure are upwards of 7m high. The metal casing has been stripped from all but the end columns. A reinforced-concrete beam also survives across the pair of columns at the west end; the others have been removed. Both end columns are 6.18m from each other (centre to centre) and the others are 3.98m apart. The two end spans were each 12m and the others 16m.					
Interest	Historical; Social; Technical					
Evaluation	This was one of the longest viaducts on the Dundalk-Clones section of the line and is still a prominent landscape feature despite the removal of the deck and abutments. It is of regional industrial heritage merit and merits inclusion in the Record of Monuments & Places.					
Significance	Regional			Action	RMP	

03817_01 FWH 23/10/2007 Viaduct from NE.



03817_02 FWH 23/10/2007 Viaduct from east.



03817_03 FWH 23/10/2007 Viaduct from SE.



03817_04 FWH 23/10/2007 Viaduct from NW.



Site 03818

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03818 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO017	Grid	252804	324916
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	The embankment has been completely removed hereabouts and the bridge over the accommodation track demolished.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03819

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03819 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO012	Grid	252268	325207
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03820

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03820 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO012	Grid	252125	325289
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03821

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03821	01	Type	Bridge (rail/road)		
County	Monaghan		OS 6-inch map	MO012	Grid	251717 325519
Surveyed?	Y		Survey date	22/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	The embankment has been completely removed and the bridge over the accommodation track demolished.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03822

History Line opened by the Dundalk & Enniskillen Railway in July 1858. The original deck was probably renewed with plate girder spans on cylindrical piers in the 1920s. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03822 01	Type	Bridge (rail/canal)			
County	Monaghan	OS 6-inch map	MO012	Grid	251635	325569
Surveyed?	Y	Survey date	22/10/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	<p>A triple-span metal beam bridge, c.45m long, formerly carried the line over the Ulster Canal (and towpath). The embankments at each end and the deck have been lifted, leaving only the supporting uprights. These comprise pairs of riveted metal cylinders, 1.85m diameter and upwards of 4m high. They are filled with concrete. The metal casing has been stripped from two of the columns. A reinforced-concrete beam also survives across each end pair of columns; the others have been removed. The columns are 5.40m from each other (centre to centre). The two end spans were each c.12m and the middle one 21m. The canal has been infilled, but a drain still runs through the west span.</p> <p>A short distance SW, across the infilled canal at the former entrance to Clonavilla House, is a discarded metal I-beam, probably from the bridge. It is c.9.5m long and 66cm x 45cm in cross-section.</p>					
Interest	Historical; Technical; Setting					
Evaluation	This is one of several multi-span viaducts in this locality. Although not on the same scale as viaducts 3813 or 3817, it is nevertheless is of technical, historical and landscape merit. It is of local industrial heritage significance.					
Significance	Local					

03822_01 FWH 23/10/2007 Viaduct from SW.



03822_02 FWH 23/10/2007 Looking west from east end of former viaduct.



03822_03 FWH 23/10/2007 Columns at east end, from SE.



Site 03823

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03823 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO012	Grid	251438	325681
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this accommodation crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Crossing used as a railcar stop from c.1935 to 1940s. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03824 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO012	Grid	251233	325799
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03824 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO012	Grid	251242	325786
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this level crossing keeper's house survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03824 03	Type	Hardstand		
County	Monaghan	OS 6-inch map	MO012	Grid	251239 325795
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces of the hardstand associated with this request halt survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Site **03825**

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03825 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO012	Grid	250898	325988
Surveyed?	N					

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959. According to Mills (1903), this bridge dates from the period 1877-1903. As the embankment would have already existed, it must have been a deck replacement.

Component	03826 01	Type	Bridge (rail/road)			
County	Monaghan	OS 6-inch map	MO011	Grid	250611	326109
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this railway bridge over a road and it associated embankment survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site **03827**

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03827	01	Type	Bridge (rail/foot)		
County	Monaghan		OS 6-inch map	MO011	Grid	250347 326171
Surveyed?	Y		Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	No traces of this subway survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03828

History Dates to 1863 with the arrival in Clones of the Ulster Railway from Portadown.

Component	03828 01	Type	Junction		
County	Monaghan	OS 6-inch map	MO011	Grid	250135 326188
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	Nothing of this junction with the Ulster Railway survives.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

History Line opened by the Dundalk & Enniskillen Railway in July 1858. The pedestrian subway (component 2) is probably of 20th century date. Line closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Component	03829 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO011	Grid	250024	326187
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this road crossing survive.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03829 02	Type	Bridge (rail/foot)			
County	Monaghan	OS 6-inch map	MO011	Grid	250015	326187
Surveyed?	Y	Survey date	11/10/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this subway are now evident.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

History

Line opened by the Dundalk & Enniskillen Railway in July 1858. Station closed to passengers by Great Northern Railway Board in October 1957 and to goods traffic by Córas Iompair Éireann in December 1959.

Old photographs show the footbridge (component 2c) to have been a substantial lattice girder bridge. The engine shed (6a) dates from c.1925 and was the first building of its type in Ireland, slightly pre-dating an identical one at Portadown. It was designed by the Indented Bar Company of London. The turntable (6b) and engine shed offices (6c) are contemporary with it. Old photographs show the signal boxes (7 and 10), which date from the period 1877-1903 (and were the work of W.H. Mills), to have been of standard GNR design – a timber cabin over brick base. The water tank (8) was a standard metal water tank on top of a stone tower.

03901_01 R.M. Arnold

Source C. Friel Collection.

Clones Station looking west.



03901_02 Unknown

Source C. Friel Collection.

Clones - arrival from Dundalk alongside the surviving platform.



03901_03 Unknown 24/08/1957 Source C. Friel Collection.
Clones Station frontage.



03901_04 Unknown Source C. Friel Collection: Real Photographs X5487.
Clones Station looking west in the late 1930s.

03901_05 T.J. Edgington/ Colour-Rail 04/1956 Source Johnston 2005, p.101.
Diesel three-unit train from Belfast at Clones Station

03901_06 M. Burnett/ Colour-Rail 08/1958 Source Johnston 2005 p.102.
LQGs No.158 from Dundalk at Clones Station.

03901_07 Unknown Source Postcard.
Commercial postcard showing Clones Station frontage.



Component	03901 01	Type	Station building; Station house			
County	Monaghan	OS 6-inch map	MO011	Grid	249867 326151	
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing remains of the station building which accommodated the waiting rooms, ticket office, parcel office, toilets, stores, and station master's living accommodation. The site has been redeveloped with a new build (Aughey Plant International). Old photos show an extensive block.					

Interest None
 Evaluation No industrial heritage significance.
 Significance Record only

Component	03901	02a	Type	Platform		
County	Monaghan		OS 6-inch map	MO011	Grid	249900 326166
Surveyed?	Y		Survey date	28/08/2007	Surveyor	AR&R
Completeness	Substantial remai		Condition	Fair	Use	Disused
Description	This was the 'up' platform serving Dundalk. Only the section to the east of the former station building now remains. It comprises a rubble stone platform wall and gravelled surface delineated with concrete kerbstones; concave moulding to edge of kerbstone. Platform terminates at west end at rubble stone wall with sandstone coping stones (the access steps came up here), and at east end at former entrance to site. There was formerly a ticket collection hut and plateway inspector's hut along it and a canopy over the section directly in front of the station building. Along the back of the platform is a random rubble wall with sandstone coping stones.					
Interest	Historical; Group					
Evaluation	This platform is a reminder of the presence of the former station building and also has group value with the other surviving buildings in the complex. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional				Action	RPS

Component	03901	02b	Type	Platform		
County	Monaghan		OS 6-inch map	MO011	Grid	249866 326172
Surveyed?	Y		Survey date	18/06/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	The platform served the line to Belfast. It was covered by an open canopy and also housed a customs office and gents' toilets. Nothing remains.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901	02c	Type	Bridge (foot/rail)		
County	Monaghan		OS 6-inch map	MO011	Grid	249889 326169
Surveyed?	Y		Survey date	18/06/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	The linked the main and island platforms.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901	03	Type	Store		
County	Monaghan		OS 6-inch map	MO011	Grid	249828 326142
Surveyed?	Y		Survey date	18/06/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	Nothing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901 04	Type	Goods shed		
County	Monaghan	OS 6-inch map	MO011	Grid	249788 326133
Surveyed?	Y	Survey date	28/08/2007	Surveyor	AR&R
Completeness	Complete	Condition	Good	Use	Industrial premises
Description	<p>Detached three-bay single-storey goods shed, situated within Clones Station. Now used by a steel fabricator business. Large modern shed extension to west end. Lean-to extension to south elevation.</p> <p>Single-span pitched slate roof supported on a series of eight trusses. Terracotta ridge tiles and replacement rainwater goods. Mono-pitched corrugated-iron roof to lean-to extension. Trusses are linked by cross ties, embedded in the side walls. Rafters and battens are concealed by timber ceiling boards. Random-coursed rubble stone walls. Western gable wall has been demolished. Pebble-dashed render to lean-to extension.</p> <p>Openings to eastern gable include a blocked up segmental-headed window opening near the apex of the gable, two blocked up window openings lower down and a large square-headed door opening with a modern steel roller door with lintel over. The door opening was enlarged, as the remains of a segmental arch can be seen on the north side. Three pairs of segmental-headed windows are located on the north elevation. Each has concrete sills and triple-light casement windows. Remains of timber-panelled canopies to south side of shed, to the east and west of the lean-to extension.</p> <p>Within the shed are two large segmental-headed openings on the south wall, now open to the interior of the lean-to extension. A third, at the east end, has been enlarged and is now a square-headed opening.</p>				
Interest	Architectural; Historical; Group				
Evaluation	This well constructed building is the last surviving building associated with the original station. Although now dwarfed by a later addition on its gable, its railway architecture is still clearly discernible. It also has group value with the engine shed. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.				
Significance	Regional			Action	RPS

Component	03901 05	Type	Cattle beach		
County	Monaghan	OS 6-inch map	MO011	Grid	249797 326110
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	No traces survive.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

Component	03901	06a	Type	Engine shed		
County	Monaghan		OS 6-inch map	MO011	Grid	249819 326225
Surveyed?	Y		Survey date	28/08/2007	Surveyor	AR&R
Completeness	Complete		Condition	Fair	Use	Industrial premises
Description	<p>Curvilinear single-storey shed arcing through 130 degrees and containing 12 radial bays ('roads') to accommodate engines. Now in use as manufacturing plant. The curvature of the building is centred on the external turntable (component 6b) by which the locos were directed to their respective roads.</p> <p>Reinforced concrete structure with a felted timber Belfast truss roof supported by concrete piers and beams. The elevation facing the former turntable has a large opening to each road, each set within oversailing eaves. Most of the openings have been infilled and new windows and doors inserted in places. The end walls are of concrete formed to a grid pattern. Their window openings are blocked-up on the SW elevation. The SE end has five timber fixed-paned window openings with concrete sills.</p> <p>The interior comprises 12 contiguous bays, each defined by reinforced concrete piers and roof beams. Concrete floor with some tracks remaining. Timber buffer stops survives to most bays with a window above each buffer. The section below each of these windows is sacrificial, designed to collapse if accidentally hit by a loco, thereby minimising damage. There is also a window opening between the sacrificial ones.</p>					
Interest	Architectural; Historical; Technical; Group; Rarity					
Evaluation	This shed was similar to the now-demolished one at Portadown and is both the earliest and only survival of its type left in Ireland. Its functional planform, reinforced-concrete walls, Belfast truss roof and sacrificial buffer walls are of architectural and technical merit. It also has group value with the nearby goods shed. It is of national industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	National				Action	RPS

Component	03901	06b	Type	Turntable		
County	Monaghan		OS 6-inch map	MO011	Grid	249832 326195
Surveyed?	Y		Survey date	18/06/2007	Surveyor	Fred Hamond
Completeness	No visible remain		Condition	N/A	Use	N/A
Description	This was used to line up locos with their relevant bay in the adjoining engine shed. Nothing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901 06c	Type	Engine shed offices			
County	Monaghan	OS 6-inch map	MO011	Grid	249778	326180
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Fair	Use	Unknown	
Description	<p>Single-storey concrete-built former engine shed office. Located adjacent to west end of engine shed. Modern lean-to extension to west end of south elevation disguises building length. Shed built to north of office.</p> <p>Roof hidden behind parapet. Concrete walls formed in grid pattern. Canted-bay window to east end elevation where foreman's office was accommodated. Central section of building was used as stores and west bay as a mess room. Square-headed window openings to south elevation with replacement windows. Square-headed window openings on north elevation blocked-up. Square-headed door opening to east end elevation with replacement timber door.</p>					
Interest	Architectural; Historical; Group					
Evaluation	This building is contemporary with the engine shed. It also has group value in the context of the station complex. It is of regional industrial heritage significance and merits inclusion in the Record of Protected Structures.					
Significance	Regional			Action	RPS	

Component	03901 07	Type	Signal box			
County	Monaghan	OS 6-inch map	MO011	Grid	249694	326142
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	This was the west signal box, of which nothing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901 08	Type	Water tank			
County	Monaghan	OS 6-inch map	MO011	Grid	249905	326196
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901 09	Type	Weighbridge			
County	Monaghan	OS 6-inch map	MO011	Grid	249908	326203
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing of this weighbridge survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03901 10	Type	Signal box			
County	Monaghan	OS 6-inch map	MO011	Grid	250040	326182
Surveyed?	Y	Survey date	18/06/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	Nothing survives.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

03901_01 AR&R 28/08/2007 East end of platform.



03901_02 AR&R 28/08/2007 Central section of platform.



03901_03 AR&R 28/08/2007 West end of platform and wall.



03901_04 AR&R 28/08/2007 Entrance gates to east of platform.



03901_05 AR&R 28/08/2007 Detail of platform kerbstone.



03901_06 FWH 18/06/2007 Goods shed, from NE.



03901_07 AR&R 28/08/2007 East gable of goods shed.



03901_08 AR&R 28/08/2007 Goods shed from SE.



03901_09 AR&R 28/08/2007 South elevation of goods shed with extensions at west end.



03901_10 AR&R 28/08/2007 East end of south elevation of goods shed.



03901_11 AR&R 28/08/2007 West end of south elevation of goods shed with canopy.



03901_12 AR&R 28/08/2007 Large shed extension to west end of goods shed.



03901_13 AR&R 28/08/2007 Steel door inserted to east gable of goods shed.



03901_14 AR&R 28/08/2007 Canopy to south elevation of goods shed.



03901_15 AR&R 28/08/2007 Canopy to west end of south elevation of goods shed.



03901_16 AR&R 28/08/2007 Segmental-headed window opening to north elevation of goods shed.



03901_17 AR&R 28/08/2007 Interior of goods shed from west end.



03901_18 AR&R 28/08/2007 Interior of goods shed from west end.



03901_19 AR&R 28/08/2007 South wall of goods shed interior.



03901_20 AR&R 28/08/2007 Round-headed opening in south wall of goods shed.



03901_21 AR&R 28/08/2007 Interior of large shed extension to west end of good shed.



03901_22 AR&R 28/08/2007 Roof trusses to good shed.



03901_23 AR&R 28/08/2007 Detail of end of truss to goods shed roof.



03901_24 AR&R 28/08/2007 Trusses and diagonal ties to roof structure of goods shed.



03901_25 FWH 18/06/2007 Engine shed from south.



03901_26 AR&R 28/08/2007 F of engine shed taken from west end.



03901_27 AR&R 28/08/2007 Front of engine shed, from east end.



03901_28 AR&R 28/08/2007 View of last bays on east end of engine shed.



03901_29 AR&R 28/08/2007 SE gable of engine shed.



03901_30 AR&R 28/08/2007 View of part of rear elevation of engine shed.



03901_31 AR&R 28/08/2007 Detail of window on SE gable of engine shed.



03901_32 AR&R 28/08/2007 SW gable of engine shed.



03901_33 AR&R 28/08/2007 Internal bays of engine shed.



03901_34 FWH 18/06/2007 Engine shed interior, looking NE.



03901_35 AR&R 28/08/2007 Engine shed: detail of arched-opening to bay.



03901_36 AR&R 28/08/2007 Engine shed: detail of recessed entrance to bay.



03901_37 AR&R 28/08/2007 Engine shed: detail of pier to interior.



03901_38 AR&R 28/08/2007 Engine shed: interior.



03901_39 AR&R 28/08/2007 Engine shed: sacrificial buffer wall.



03901_40 AR&R 28/08/2007 Engine shed: detail of buffer.



03901_41 AR&R 28/08/2007 Engine shed office.



03901_42 FWH 18/06/2007 Engine shed offices, from SE.



03901_43 AR&R 28/08/2007 Engine shed office: east gable.



03901_44 AR&R 28/08/2007 Engine shed office: west gable.



03901_45 AR&R 28/08/2007 Lean-to extension on south elevation of engine shed office.



03901_46 AR&R 28/08/2007 North elevation of engine shed office within modern shed.



History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed by Great Northern Railway Board in September 1957.
The level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

Component	03902 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO011	Grid	249498	326025
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this road crossing survives. It is also shared with the Ulster Railway (7502.1)					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Component	03902 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO011	Grid	249501	326037
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Good	Use	Dwelling	
Description	A greatly refurbished and enlarged former level crossing keeper's house on the NE side of the road crossing. It was originally a two-bay, two-storey house aligned east-west along the line. According to the present owner, the ground floor was of random rubble and the first floor of brick, with a central chimney. It was derelict by 1972 when he purchased it. A third bay was added to the east gable and two rear returns, all two storeys high. There is also an open veranda along the length of the south elevation. The present gabled roof is of artificial slate; according to the owner, the original sarked common rafters survive. The walls are cement rendered. The door and window openings (all of which are square-headed openings) have been enlarged and replacement frames and concrete cills inserted. This house was also shared with the Ulster Railway line.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

03902_01 FWH

23/08/2007 Refurbished house and former crossing from SW.



Site 03903

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed by Great Northern Railway Board in September 1957.

Component	03903 01	Type	Level crossing		
County	Monaghan	OS 6-inch map	MO011	Grid	249301 325915
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond
Completeness	Some remains	Condition	Fair	Use	Disused
Description	A wrought-iron sunburst gate and metal post survive on the north side of the line. The post appears to be a section of railway line, which curved flanges along the bottom. The post has been concreted into the ground and has probably been relocated from its original position. This crossing was also shared with the Ulster Railway.				
Interest	None				
Evaluation	No special industrial heritage significance.				
Significance	Record only				

03903_01 FWH 23/08/2007 Gate from south.



Site 03904

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed by Great Northern Railway Board in September 1957.

Component	03904 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO011	Grid	249090	325819
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	A pair of wrought-iron sunburst gates are hung from metal posts on the south side of the former line. There is also a gate and post on the north side, but aligned at right angles (probably not its original alignment). The posts are sections of cast-metal rail. This crossing was also shared with the Ulster Railway.					
Interest	None					
Evaluation	Because one gate is no longer in its original position, this crossing is no longer of any special industrial heritage significance.					
Significance	Record only					

03904_01 FWH 23/08/2007 Gate from NW.



Site 03905

History Line opened by the Dundalk & Enniskillen Railway in July 1858. This divergence dates to the opening of the Cavan extension in 1862. Closed by Great Northern Railway Board in September 1957.

Component	03905 01	Type	Junction		
County	Monaghan	OS 6-inch map	MO011	Grid	249060 325808
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond
Completeness	No visible remain	Condition	N/A	Use	N/A
Description	From Clones Station, the Dundalk-Enniskillen line ran parallel with the Ulster Railway before the latter diverging here to Cavan. No traces survive, the ground having been re-landscaped as tennis courts at the back of later houses.				
Interest	None				
Evaluation	No industrial heritage significance.				
Significance	Record only				

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed by Great Northern Railway Board in September 1957.
The level crossing house (component 2) is cited as watch house on 2nd edition OS six-inch map.

Component	03906 01	Type	Level crossing			
County	Monaghan	OS 6-inch map	MO011	Grid	248747	325715
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	Some remains	Condition	Fair	Use	Disused	
Description	A wrought-iron sunburst gate survives on the north side of this accommodation crossing. It is hung from a later steel joist post and is not in its original position.					
Interest	None					
Evaluation	No special industrial heritage significance.					
Significance	Record only					

Component	03906 02	Type	Level crossing house			
County	Monaghan	OS 6-inch map	MO011	Grid	248759	325727
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	Complete	Condition	Poor	Use	Disused	
Description	A two-bay, single-storey house aligned east-west at the NE side of this accommodation crossing. It has a gabled natural slate roof with central brick chimney. The walls are lime rendered. All openings are square headed. There is a small entrance porch and window to the west gable and two windows to the south elevation (one per bay). There is a small lean-to on the east gable. There are no openings on the north (rear) elevation. All the windows are 2/2 sliding sashes and have concrete cills. No internal access.					
Interest	Historical					
Evaluation	This house is one of the few relatively unaltered ones along the Co Monaghan section of the Irish North-West railway. Its rarity value and historical association with the railway make it of local industrial heritage significance.					
Significance	Local					

03906_01 FWH 23/08/2007 Level crossing gate.



03906_02 FWH 23/08/2007 House from SE.



03906_03 FWH 23/08/2007 House from north. Note gate at right.



03906_04 FWH 23/08/2007 House from west.



Site 03907

History Line opened by the Dundalk & Enniskillen Railway in July 1858. Closed by Great Northern Railway Board in September 1957.

Component	03907 01	Type	Water pump (house)			
County	Monaghan	OS 6-inch map	MO011	Grid	248604	325693
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	No visible remain	Condition	N/A	Use	N/A	
Description	No traces of this pump house survive. The embankment has also been removed hereabouts.					
Interest	None					
Evaluation	No industrial heritage significance.					
Significance	Record only					

Site 03908

History Line opened by the Dundalk & Enniskillen Railway in July 1858. The original deck was probably renewed with plate girder spans on cylindrical piers in the 1920s. Closed by Great Northern Railway Board in September 1957.

Component	03908 01	Type	Bridge (rail/river)			
County	Monaghan	OS 6-inch map	MO011	Grid	248500	325669
Surveyed?	Y	Survey date	23/08/2007	Surveyor	Fred Hamond	
Completeness	Traces	Condition	Poor	Use	Disused	

Description A pair of cast-iron columns on the east side of this tributary of the Finn River are all that survive of this metal beam railway bridge. Each column is 1.91m in diameter and stands 3.61m high. They are 8.08m apart (centre to centre). Across their tops is a riveted metal girder clad in concrete. The lines were probably affixed to the top flange of this beam. The embankment has been completely removed on the Co Monaghan side of the river. Although it survives on the Co Fermanagh side, no columns appear to survive.

Interest Historical; Technical

Evaluation This pair of columns is now the only indication of a once relatively substantial metal bridge. This coupled with their railway associations make them of local industrial heritage significance.

Significance Local

03908_01 FWH 23/08/2007 Columns from NE.



